

OTAY RIVER BUSINESS PARK

SPECIFIC PLAN

December 2017

OTAY RIVER BUSINESS PARK SPECIFIC PLAN

December 2017

Prepared For:

City of Chula Vista
276 Fourth Avenue
Chula Vista, CA 91910

Applicant:

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CHAPTER 1 - INTRODUCTION

The Otay River Business Park is a Specific Plan for an approximate area of 52.87 acres located just north of the existing City of Chula Vista boundary in southern San Diego County. The property is situated within the City's Southwest Planning Area along the north side of the Otay River, east of Interstate 5 and west of Interstate 805. The property is also located within the boundaries of the Otay Valley Regional Park (OVRP). The project location is shown on Exhibit 1 Regional Map and Exhibit 2 Vicinity Map.

The Otay River Business Park Specific Plan (the "Plan" or "ORBP") envisions the development of a high quality industrial business park that will enhance the opportunities for quality, contemporary business uses with ancillary support commercial services, complementary to its surroundings. The Specific Plan establishes the land use, intensity, development regulations, design standards, and primary infrastructure components that will support future development of the project.

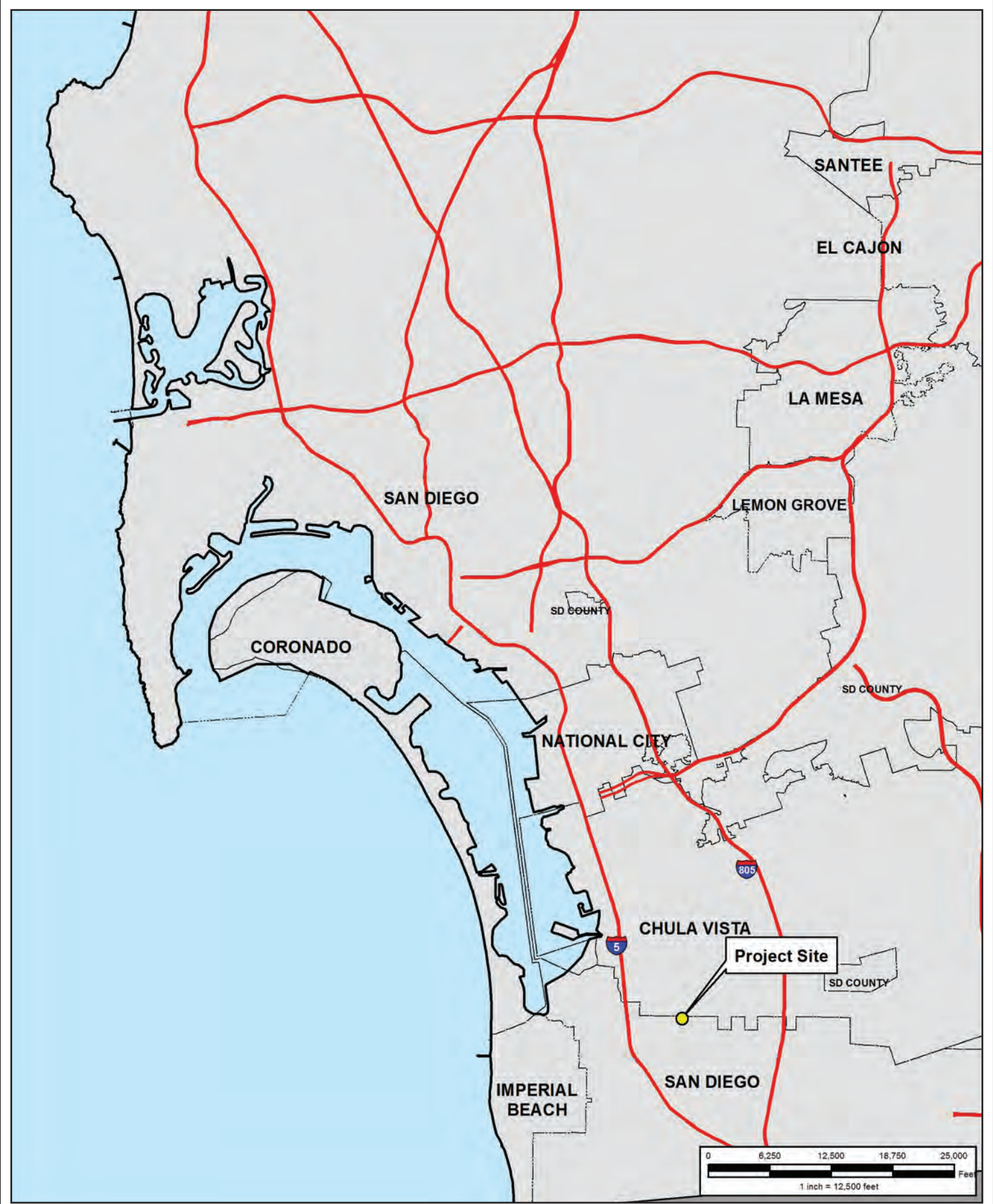
The Specific Plan proposes industrial and commercial lots served by new street connections to the existing street system. The Plan incorporates an area for a future Community Park, as designated by the 2005 General Plan of the City of Chula Vista in this area, and is situated adjacent to and above the Otay River Valley.

The majority of the Site has been graded or disturbed, with both paved and unpaved parking, driveways and roads throughout the parcels, various buildings and structures associated with the existing heavy industrial uses, former concrete batch operations, and former agricultural field activities. The Otay River Business Park will provide a new development pattern at this site with business park uses and complementary amenities for the existing community.

1.1 Specific Plan Location and Boundaries

The property within the Otay River Business Park Specific Plan (the "Site") is located in the southwestern portion of the City of Chula Vista, south of Main Street between Interstates 5 and 805, at Main Street and Fourth Avenue. It is within the Main Street District, which functions as a commercial-industrial service corridor, with residential neighborhoods located generally north of the corridor, and the Otay River Valley open space to the south. The Site is bordered on the north by Main Street, on the west by Broadway/Beyer Boulevard, on the east by Fourth Avenue, with the Otay River along the southern boundary. It is located approximately 1 mile east of Interstate 5 and 2 miles west of Interstate 805, and consists of multiple contiguous parcels of land.

Exhibit 1 Regional Map



REGIONAL MAP



OTAY RIVER BUSINESS PARK
SPECIFIC PLAN

Source: SANGIS

EXHIBIT
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1.2 Authority and Requirements

California Government Code Section 65450 *et seq.* grants local planning agencies the authority to prepare Specific Plans for any area covered by a General Plan, as a tool for the implementation of the General Plan. A Specific Plan can effectively establish a link between implementing policies of the General Plan and an individual development proposal in a defined area.

The Otay River Business Park Specific Plan has been prepared in accordance with State Specific Plan requirements under the authority of Government Code Sections 65450-65457 and consistent with the State guidelines for the preparation of Specific Plans. This Specific Plan contains sections as required that address the following:

1. The distribution, location and extent of the use of land within the area covered by the plan. (Refer to Chapter 4)
2. The proposed distribution, location, extent, and intensity of major components of infrastructure (transportation, sewage, water, drainage, solid waste disposal, and other essential facilities) proposed to be located within the area covered by the Plan and needed to support the land uses described in the Plan. (Refer to Chapter 6)
3. The standards and criteria by which development will proceed, and standards for the conservation, development and utilization of natural resources, where applicable. (Refer to Chapters 4 and 5)
4. Implementation measures including regulations, programs, public works projects, and financing measures. (Refer to Chapter 7)
5. A statement of the relationship of the Specific Plan to the General Plan. (Refer to Chapter 3)

This Specific Plan details the land use proposal for the Otay River Business Park. The Plan discusses intensity, project character, open space, compatibility with surrounding development, and development design guidelines.

1.3 Purpose and Intent of the Specific Plan

The Otay River Business Park Specific Plan has been prepared to implement the City of Chula Vista General Plan, and provides a comprehensive working document that describes guidelines and standards for implementation of the development. The document provides a detailed description of proposed land use and infrastructure requirements. Design and development standards have been prepared to assist in creating a business park that provides upgrades and improvements from the prior site land uses, in a way that complements the surrounding community.

Careful consideration has been taken in terms of scale, character, intensity, lot size, architectural and site development standards for the Specific Plan. The key objectives are to provide a business park incorporating industrial, professional office,

and commercial uses to create economic opportunities in the area, provide land for future park uses and respect the Otay River floodway and Otay River Valley open space that is part of the adjacent Otay Valley Regional Park. Goals for the Otay River Business Park Specific Plan are established as follows:

1. Create a business park that respects the character of its surroundings, incorporates a quality design for attractive buildings and street frontages, provides locations for local businesses to operate, and is an asset to the City and region.
2. Promote a quality development consistent with the goals and policies of the Chula Vista General Plan and Design Standards for the Main Street District.
3. Provide for comprehensive planning that assures the orderly development of the site in relation to its surroundings.
4. Establish development and design regulations providing for high quality and visibly attractive landscaping and building architecture.
5. Protect environmentally sensitive areas within the Specific Plan boundaries, and designate usable area for a future community park adjacent to the Otay Valley River Regional Park that can link to existing OVRP trails.
6. Develop a Plan that is economically feasible and able to be implemented based on anticipated economic conditions such that no economic burden to the City of Chula Vista would occur.

1.4 Relationship of the Specific Plan to the Chula Vista Municipal Code

The Otay River Business Park Specific Plan conforms to and implements the Chula Vista General Plan. It is a legislative document including a new set of zoning standards for the property that, in concert with other portions of the Municipal Code, will be applied to new project development. Where there is a conflict between the provisions of this Specific Plan and other sections of the Chula Vista Municipal Code, the provisions of this Specific Plan shall prevail. Where this Specific Plan does not address a standard or regulation, the existing provisions of the municipal code shall apply.

The Otay River Business Park Specific Plan will be reviewed by the City of Chula Vista and will ultimately be considered by the Planning Commission and City Council. The Council will have final review and approval authority.

1.5 Construction of Language

As used in this Specific Plan, the terms “shall” means mandatory or required; “should” means recommended or desired; and “may” means optional.

Where stated, numerical metrics take precedence over graphic metrics.

1.6 Severability

If any term, provision or condition of this Specific Plan is determined invalid, void or unenforceable, the remainder of this Specific Plan shall not be affected thereby to the extent such remaining provisions are not rendered impractical to perform, taking into consideration the purposes of this Specific Plan.

CHAPTER 2 - PLANNING CONSIDERATIONS

2.1 Existing Land Use & Zoning Prior to Adoption of the Specific Plan

The Otay River Business Park area is currently regulated by the City of Chula Vista land use and zoning standards. The 52.87-acre property is within the Southwest Planning Area and the Main Street District, and designated as Limited Industrial by the Chula Vista General Plan, with a designation for a future Community Park in the vicinity of this property. Land use within the Otay River is designated Open Space. Zoning on this site is ILP-Limited Industrial with a Precise Plan Overlay, the purpose of which is to allow for certain industrial uses but limiting others that create a greater nuisance or impact on nearby residents and businesses.

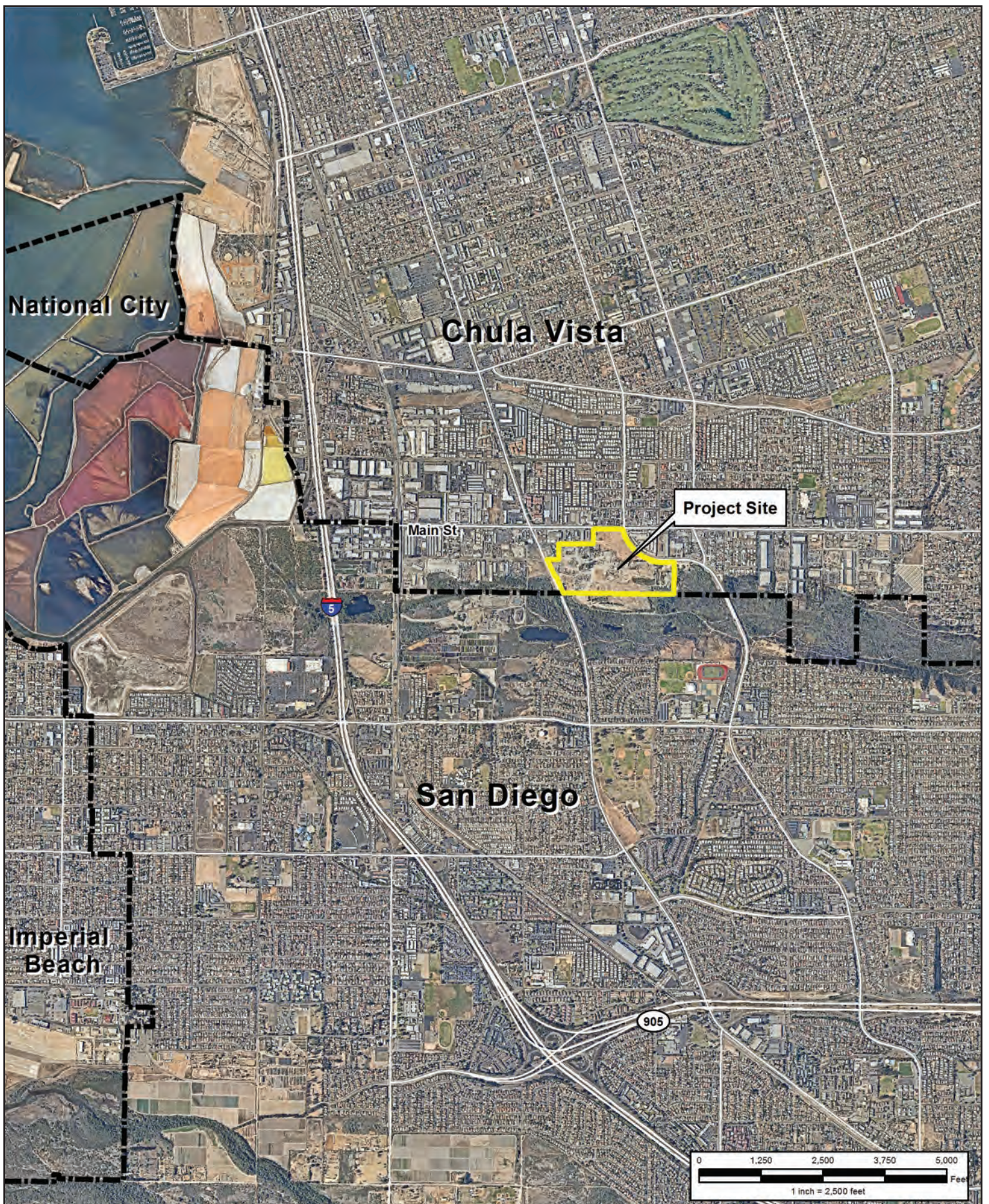
The Main Street Corridor between Interstate 5 and Interstate 805 is currently dominated by a mixture of commercial and light industrial uses with several areas, including part of the Specific Plan site, used for heavy industry.

The Specific Plan site is located within the Otay Valley Regional Park (OVRP) boundaries. The OVRP was established in the early 1990's between the City of Chula Vista, City of San Diego and County of San Diego and extends over 11 miles along the Otay River from the southeastern edge of the South Bay Wildlife Refuge through the Otay River Valley to the land surrounding the Otay Lakes area. The OVRP Concept Plan and the Design Standards and Guidelines are the documents that provide the framework for direction and guidance for parkland acquisition, development efforts, and evaluations of private development that may potentially affect the OVRP. These documents provide direction related to trails and landscape treatment for improvements within public lands as well as private development that is located within the OVRP. The Concept Plan does not preclude development of private lands that are located within its boundaries. The Otay River Business Park Specific Plan was prepared using the guidelines for the OVRP and incorporates the guidelines for private development in the developed portion of the site, and the design standards for the future park area and portions of the developed site that face the future park and the Otay River Valley.

Exhibit 2 Vicinity Aerial Map

2.2 Site History and Current Property Use

The Specific Plan site uses have included both mining/industrial uses and agricultural uses for many decades. About one third of the property, in the area immediately south of Main Street, is characterized by former agricultural row-crop fields, which had been one of the last remnants of the broader agricultural operations throughout this area during the early to mid-1900s. The majority of the property includes disturbed and developed areas associated with the former heavy industry activities of sand mining and concrete batch plant operations, begun by 1928. Batch plant operations expanded, with associated structures built by 1948. Additional administration buildings and aggregate conveyers were added in the 1960's and



VICINITY MAP



OTAY RIVER BUSINESS PARK
SPECIFIC PLAN

Source: SANGIS,
nearmap imagery

EXHIBIT
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1970's and the operations remained until the mid-2000's. In the westerly portion of the property were various industrial businesses, such as auto repair facilities and sales of marine and vehicular items constructed during the 1960's-1970's, all of which are no longer in operation and have been removed from the site.

The majority of the Site has been graded or disturbed, with both paved and unpaved parking, driveways and roads throughout the parcels, various structures associated with the former uses, including the concrete batch operations.

2.3 Surrounding Land Uses

The northwest corner of the area between Broadway and 7th Avenue and south of Main Street contains developments outside the Specific Plan comprising various commercial establishments including a gas station, repair shops, a thrift store, and one apartment complex across 7th Avenue from the Site along Main Street.

Otherwise, Main Street is largely industrial and commercial in character, with residential neighborhoods located to the north of this corridor. Commercial uses focused on automotive repair and sales are across Main Street from the Site to the north.

East of the site across Fourth Avenue are commercial uses and a self-storage facility. Adjacent to the Site between Fourth Avenue and the Otay River is open space property owned by the City of Chula Vista.

Additional commercial and light industrial uses, along with an aggregate concrete plant, are across Broadway. South of the site is a previously graded portion of the former concrete batch plant operation that is within the City of San Diego boundary and not included in the Specific Plan. The Otay River channel is south of this parcel and contains dirt walking paths. No other development is present in the channel.

Exhibit 3 Zoning Map

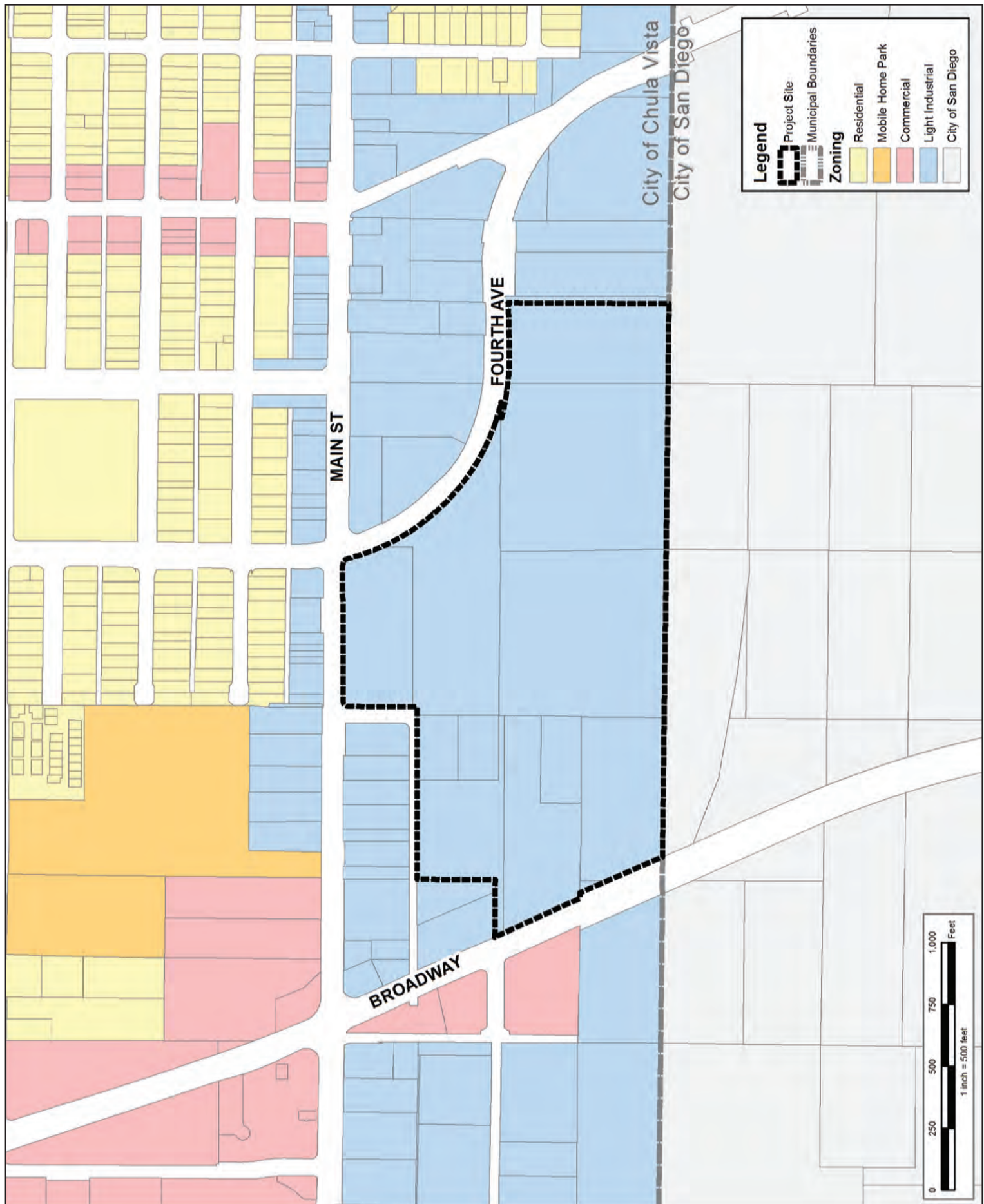
2.4 Physical Setting

A. Topography

The site has been previously graded, and slopes generally from the northeast, at about 55 feet above mean sea level (AMSL), down to the southwest corner, at about 32 feet AMSL.

Exhibit 4 Existing Topography

The proposed mass grading plan for this Specific Plan establishes future building pad areas, new street alignments, major stormwater systems, and elevates the building areas for future structures at least one foot above the 100-year floodplain. North of Faivre Street, the property will retain a high point of about 55 feet AMSL in the northeast corner, sloping down to 50 feet AMSL to



Legend

- Project Site
- Municipal Boundaries

Zoning

- Residential
- Mobile Home Park
- Commercial
- Light Industrial
- City of San Diego



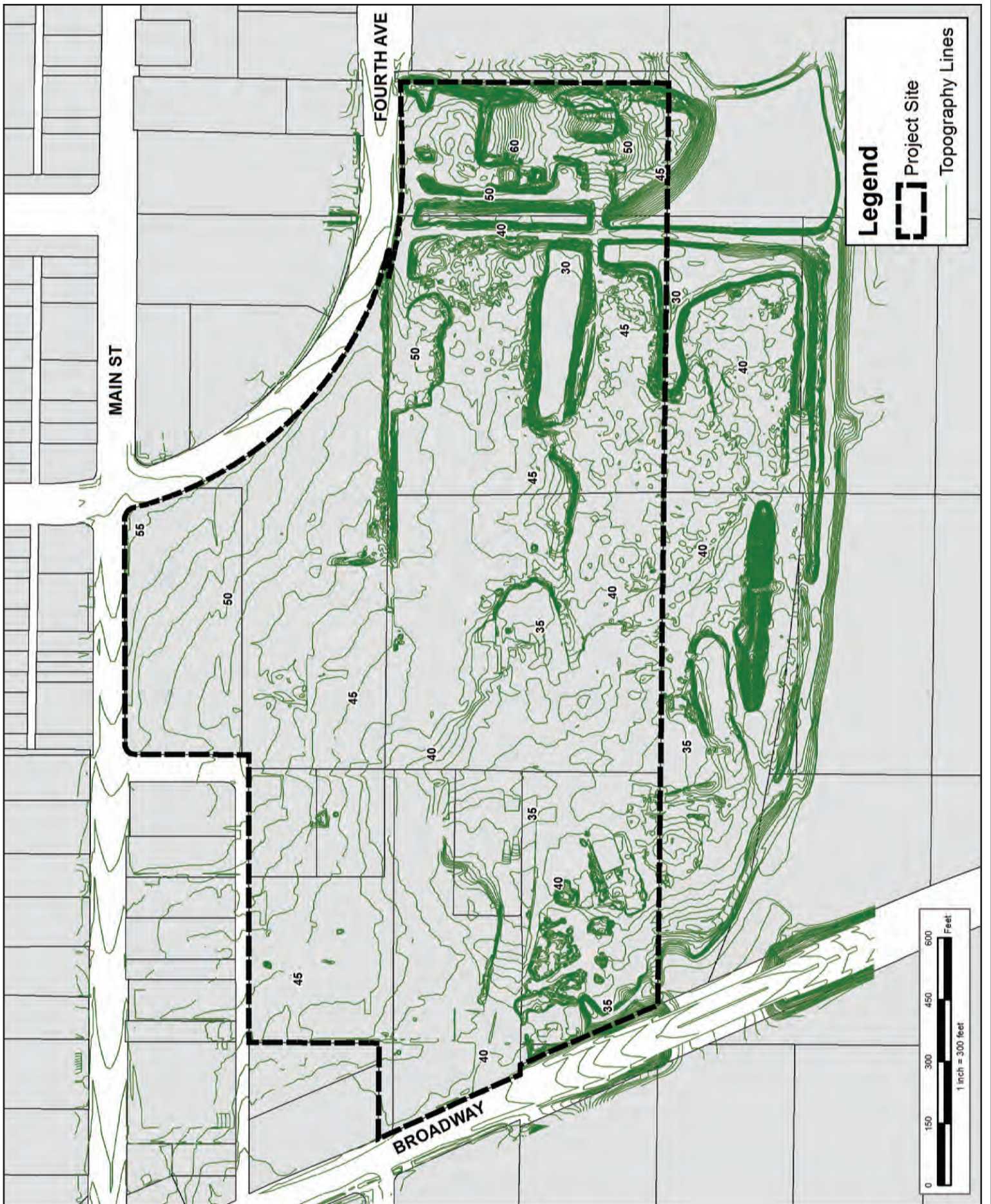
ZONING EXHIBIT



OTAY RIVER BUSINESS PARK
SPECIFIC PLAN

Source: SANGIS,
City of Chula Vista

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the proposed desilt basin #9. South of Faivre Street, the proposed grading starts at about 50 feet AMSL on the eastern portion adjacent to the future park space and drops to about 45 feet AMSL near the southwest corner of the site, with a slope along the southern boundary of the graded portion of the property.

Exhibit 5 Mass Grading Plan

B. Soils/Geology

In the central and southern portion of the site, undocumented fill consisting of a wide variety of fine to very coarse material is present at about 10 to 30 feet thick. Under this undocumented fill is a layer 20 to 30 feet thick of young alluvium.

Groundwater is present in lower elevations about 10-15 feet below surface and may require soil stabilization.

The Site is about 5 miles east of the Newport-Inglewood/Rose Canyon Fault Zone and no faults (active or inactive) traverse the Site. Current soil conditions are at moderately high risk of liquefaction due to the soil conditions and presence of near-surface groundwater, and will require additional investigation prior to the development of a precise grading plan.

Exhibit 6 Soils Exhibit

C. Hydrology and Drainage

Portions of the site are within the Otay River floodway and floodplain. The elevation of the southern portion of the Specific Plan site will be raised so that the final graded building pads would be out of the existing floodplain.

The Savage Dam on Lower Otay Lake is located approximately 10 miles to the east, and a dam breach would inundate extensive areas downstream. The entire Specific Plan site is within the inundation area that would be associated with a breach of the Savage Dam at Lower Otay Lakes.

Exhibit 7 Flood Zone Exhibit

D. Biological Resources

The Site is located within the City of Chula Vista Multiple Species Conservation Program (MSCP).

Within the Site, the following vegetation communities were found based on the current survey by BLUE Consulting Group:

1. Southern Willow Scrub: Approximately 0.33 acres onsite adjacent to the southern, southwestern boundary, and offsite adjacent to the eastern property lines.



* This is a schematic illustration of the proposed grades. Detailed grading information is provided on the grading plans and TPM by K&S Engineering dated March 2017.



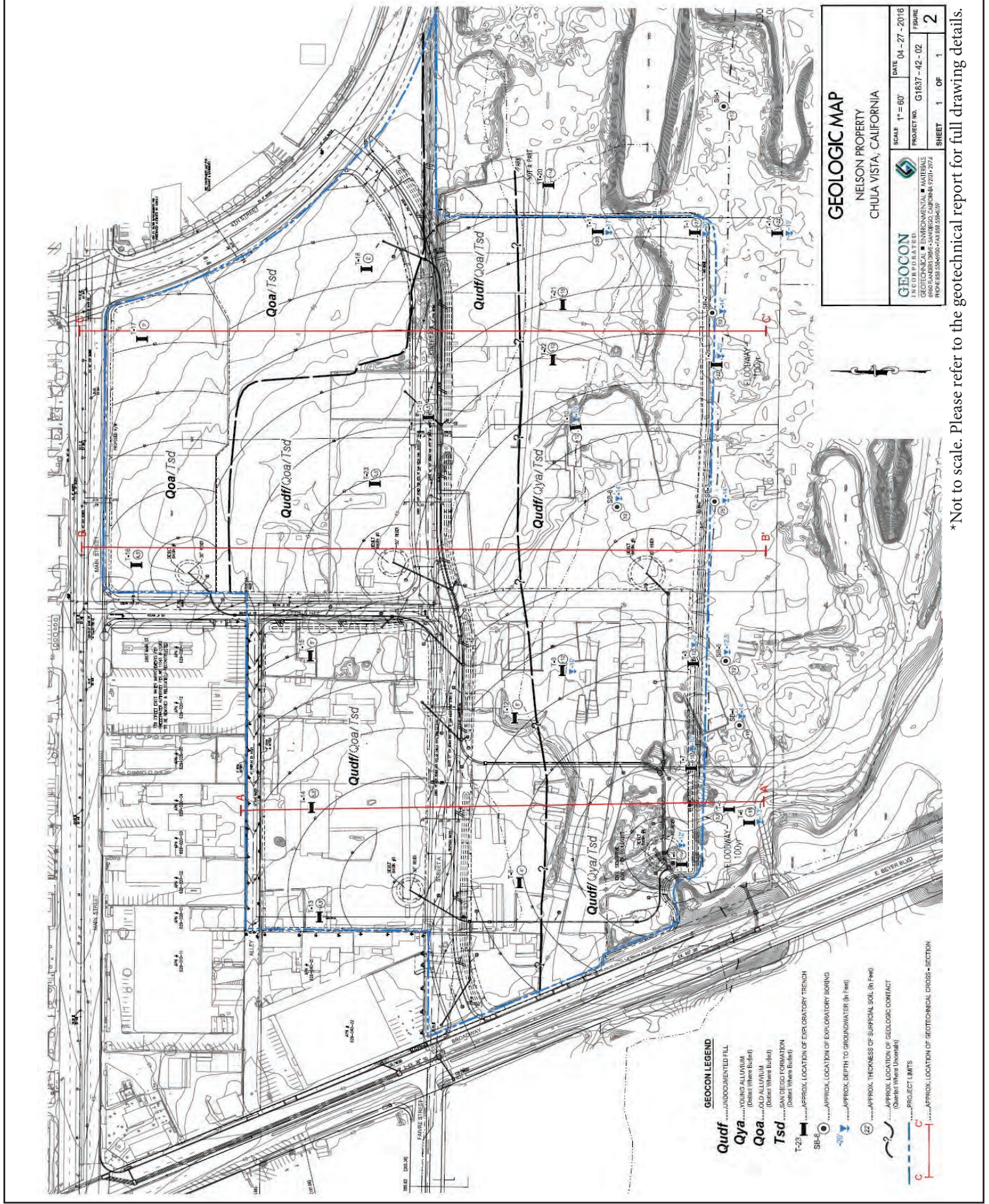
THE LIGHTFOOT PLANNING GROUP
MASS GRADING PLAN



OTAY RIVER BUSINESS PARK
SPECIFIC PLAN

Source: SANGIS 8-15, K&S Engineering

EXHIBIT
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GEOLOGIC MAP
 NELSON PROPERTY
 CHULA VISTA, CALIFORNIA

GEOCON
 INCORPORATED
 10000 WILSON AVENUE, SUITE 100
 CHULA VISTA, CALIFORNIA 92011-2074
 PHONE 619.594.9000 FAX 619.594.9197

SCALE 1" = 60'
 DATE 04-27-2016
 PROJECT NO. G1637-42-02
 SHEET 1 OF 2



- GEOCON LEGEND**
- Quaff** UNDOCUMENTED FILL
 - Qya** YOUNG ALLUVIUM (Dikes Where Buffer)
 - Qoa** OLD ALLUVIUM (Dikes Where Buffer)
 - Tsd** SAN DIEGO FORMATION (Dikes Where Buffer)
 - T-20 APPROX. LOCATION OF EXPLORATORY TRENCH
 - SBE-5 APPROX. LOCATION OF EXPLORATORY BORING
 - 20' APPROX. DEPTH TO GROUNDWATER (In Feet)
 - 7' APPROX. THICKNESS OF SURFICIAL SOIL (In Feet)
 - APPROX. LOCATION OF GEOLOGIC CONTACT (Quarrel Where Uncertain)
 - PROJECT LIMITS
 - C-C APPROX. LOCATION OF GEOTECHNICAL CROSS-SECTION

*Not to scale. Please refer to the geotechnical report for full drawing details.



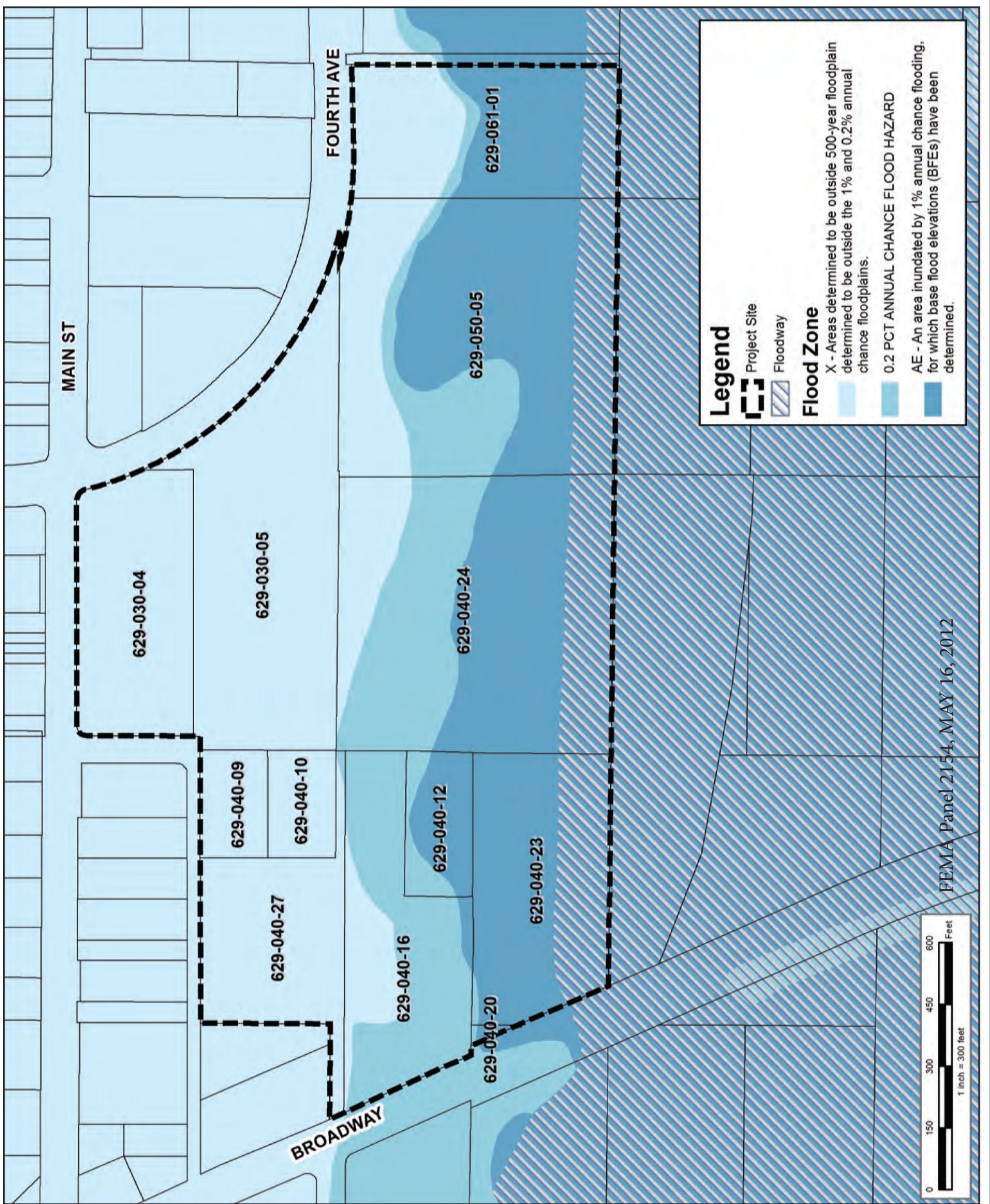
SOILS EXHIBIT



OTAY RIVER BUSINESS PARK
 SPECIFIC PLAN

Source: GEOCON, Incorporated

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FEMA Panel 2154, MAY 16, 2012



2. Eucalyptus Woodland: Approximately 0.5 acres onsite around the flood control channel.
3. General Agriculture: Approximately 8.61 acres of former agricultural fields at the northern portion of the Site.
4. Arundo Scrub: Approximately 0.74 acres within the Southern Willow Scrub community, around the retention basins, and within the flood control channel.
5. Tamarisk Scrub: Approximately 0.05 acre near the southern edge of the property.
6. Freshwater Marsh: Approximately 1.66 acres within the former mining basins.
7. Urban/Developed Land: Approximately 41.03 acres.

The Southern Willow Scrub in the southwest corner and Freshwater Marsh in portions of the future park area are considered Sensitive Plant Communities, but no sensitive plant species were observed onsite. No sensitive animal species were observed onsite except for one Yellow Warbler bird which is a Species of Special Concern. It was observed in one of the former mining basins in the future park area. Chapter 4, Section 4.6 of this Specific Plan establishes criteria to limit grading in and adjacent to these areas.

Urban/developed areas and agricultural areas onsite lack the resources to support wildlife, and only birds and evidence of mammals common to urban areas and the type of surrounding vegetation are present. Mature trees onsite provide a potential for nesting sites although no sensitive bird or mammal species are known to be present. Potential habitat for reptiles and amphibians is possible in the retention basins, depending on water flows, but none are currently present. No vernal pools or potential invertebrate habitat are onsite.

Exhibit 8 Biology Exhibit



- Urban/Developed Habitat
- Agriculture
- Eucalyptus Woodland
- Non-Native Vegetation
- Property
- Upland Basin
- Tamarisk Basin
- Arundo
- Southern Willow Scrub
- Freshwater Marsh Basin
- Flood Control Channel
- YWAR
- Temp. Slope Impacts
- Impact Footprint
- Biological Open Space
- Open Space - Lot B
- Future City Park Footprint



CHAPTER 3 - GENERAL PLAN VISION, GOALS, OBJECTIVES, AND POLICIES

The Chula Vista General Plan sets forth a series of goals, objectives and policies for the attainment of the community's collective vision of the City. A summary of how this Otay River Business Park Specific Plan achieves or helps to implement the goals, objectives, and policies of the General Plan, and therefore is consistent with the applicable goals and policies of the General Plan, is presented in this Chapter. To reduce the length of this General Plan Consistency Analysis, this Chapter does not assess Specific Plan consistency with every goal or policy included in the adopted General Plan. Instead, it focuses on the goals and policies relevant to the land uses and development within the Specific Plan area

Eight major themes support Chula Vista's vision for the future and provide a foundation for the policies and action programs of the General Plan. These themes represent the City's core values, which are also reflected in the Otay River Business Park Specific Plan (ORBSP) as summarized below:

Theme 1 Strong Community Character and Image – The ORBSP is adjacent to the Otay River, one of the unique physical features of the community that provide open space, trails, and recreational opportunities within the Otay Valley Regional Park (OVRP). The Specific Plan will enhance the interface in this location through designation of land for a future community park, protection of the floodway as open space, and sensitive stormwater management and landscaping to enhance the river interface. The Specific Plan design also implements the design program of the Main Street Streetscape Master Plan which will enhance the character of this corridor and neighborhood.

Theme 2 Healthy and Sustainable Economy – The Specific Plan will accommodate expansion of the local economy by providing for a broad range of business and employment opportunities, along with the improvement of local facilities and services to enhance quality of life. Future development in accordance with the ORBSP will provide public facilities concurrent with need. Development under the ORBSP represents an investment in new uses for this former mining site that will create opportunities to attract new businesses to enhance the City's economic vitality.

Theme 3 Strong and Safe Neighborhoods – The ORBSP will create new employment opportunities and businesses within the community, along with new street connections through the area, and will set aside land for a future park in a neighborhood that has been identified in the General Plan to need additional park acreage. The new ORBP will help create an appealing area to shop, work and play, enhancing the nearby neighborhood.

Theme 4 Improved Mobility – The ORBSP provides for new street connections, incorporating landscaped sidewalks and bike lanes to serve pedestrians and bicyclists connecting to existing and future facilities and transit service in the immediate vicinity. The design elements of the Main Street Streetscape Master Plan will be implemented along the project frontage for this major east/west corridor.

Theme 5 Healthy and Sustainable Environment – The ORBPSP is located adjacent to the Otay River Valley greenbelt and within the OVRP, and addresses protection of the floodway in permanent open space, transitional vegetation for protection of nearby habitat areas, as well as state of the art stormwater protections. The mix of uses, including industrial, commercial and future park uses, along with protection of floodway areas of the Otay River, will contribute to a healthy community by protecting resources and providing balanced, connected and sustainable land uses.

Theme 6 High Quality Community Services – The Specific Plan incorporates the City's requirements to ensure that services and infrastructure expand to match needs created by growth and redevelopment.

Theme 7 Effective Growth Management and Plan Implementation – The ORBPSP refines the identified General Plan goals for industrial uses at this site, incorporating elements to serve a variety of community interests, including nearby employees, residents and businesses. This includes a mix of land uses that provide for jobs, services and shopping along with recreation (future park site, bicycle and pedestrian linkages).

Theme 8 Shaping the Future Through the Present and Past - The ORBPSP includes landscape design features, including project corners landscaped to simulate the appearance or row crops and the inclusion of signage recognizing the agricultural history of the site, to respect and acknowledge the former agricultural heritage in this area. Additionally, the Specific Plan provides for implementation of the Streetscape Master Plan for Main Street, and addresses compatible land uses and edge transitions to existing and future uses, including the OVRP and future park site.

In order to implement the City's vision and themes, specific objectives and policies are established by the General Plan. The following discussion lists and describes those that are being implemented by the Otay River Business Park Specific Plan.

3.1 Land Use and Transportation

Objective LUT-1

Provide a balance of residential and non-residential development throughout the City that achieves a vibrant development pattern, enhances the character of the City, and meets the present and future needs of all residents and businesses.

Policies:

LUT 1.5 Endeavor to create a mixture of employment opportunities for citizens at all economic levels.

LUT 1.6 Attract and maintain land uses that generate revenue for the City of Chula Vista, while maintaining a balance of other community needs, such as housing, jobs, open space, and public facilities.

LUT 1.15 Allow office uses that are associated with complementary commercial service businesses in commercial service areas.

The Otay River Business Park Specific Plan allows for the development on the site of a former aggregate mine and concrete batch operation, heavy industrial uses and agricultural uses, into a Business Park that will provide for both commercial and light industrial development areas. Planned land uses may consist of a variety of industrial, office, service, retail, and food services that would provide employment opportunities for residents with a range of skills and education. The Specific Plan land uses include reserving a portion of the site and making it available for purchase by the City of Chula Vista for use as a public park, consistent with the General Plan land use map, in an area that can provide community recreation amenities, and complement existing open space within the Otay River Valley.

Objective LUT-6

Ensure adjacent land uses are compatible with one another.

Policies:

LUT 6.1 Ensure, through adherence to design guidelines and zoning standards, that the design review process guarantees excellence in design and that new construction and alterations to existing buildings are compatible with the best character elements of the area.

The Specific Plan establishes customized design guidelines and development standards for the ORBP to set the required and recommended characteristics of future development to ensure a quality project. These incorporate elements consistent with the Main Street Streetscape Master Plan, recognize the proximity to the Otay River with transitional buffer requirements, and will support a quality Business Park environment.

Objective LUT-7

Appropriate transitions should be provided between land uses.

Policies:

LUT 7.4 Require landscape and/or open space buffers to maintain a naturalized or softer edge for proposed private development directly adjacent to natural and public open space areas.

The Otay River Business Park Specific Plan includes an area set aside for future purchase by the City of Chula Vista for a park site. Landscaping is required between the proposed Business Park and the park planning area, and on both sides of any proposed walls along this boundary. The Specific Plan area is also adjacent to a parcel to the south in the City of San Diego zoned Open Space Floodplain. The southern portion of the Specific Plan directly adjacent to the City boundary is a planning area designated as a floodway, with limited open space uses allowed (per Chula Vista Municipal Code, Section 19.50.040). Between the floodway and the building pads is a slope that is to be planted with native plant species appropriate for a slope as a transitional buffer between the River and the business park. Landscaping is required on both sides of any wall that is proposed along the top of this slope.

Objective LUT-10

Create attractive street environments that complement private and public properties, create attractive public rights-of-way, and provide visual interest for residents and visitors.

Policies:

LUT 10.5 Require undergrounding of utilities on private property and develop a priority based program of utility undergrounding along public rights-of-way.

Power poles within the Specific Plan boundaries or directly adjacent to the Plan boundaries are required to be undergrounded by the Site developer, except where the poles are providing power to existing uses.

Objective LUT-11

Ensure that buildings and related site improvements for public and private development are well-designed and compatible with surrounding properties and districts.

Policies:

LUT 11.2 Promote and place a high priority on quality architecture, landscape, and site design to enhance the image of Chula Vista, and create a vital and attractive environment for businesses, residents, and visitors.

LUT 11.3 The City shall, through the development of regulations and guidelines, ensure that good project landscape and site design creates places that are well-planned; attractive; efficient; safe; and pedestrian-friendly.

LUT 11.4 Actively promote architectural and design excellence in buildings, open space, and urban design.

The Otay River Business Park Specific Plan sets forth standards governing site layout, building design, and landscape design in order to create a high quality project along the Main Street commercial corridor.

Objective LUT-18

Reduce traffic demand through Transportation Demand Management (TDM) strategies, increased use of transit, bicycles, walking, and other trip reduction measures.

Policies:

LUT 18.3 Provide and enhance all feasible alternatives to the automobile, such as bicycling and walking, and encourage public transit ridership on existing and future transit routes.

LUT 18.9 Adopt roadway design guidelines that enhance street connectivity for pedestrians.

The Specific Plan supports alternative transportation by including the requirement for bicycle lanes on both sides of all interior streets, and along the project boundary on Fourth Avenue and Main Street available to connect to future off-site bicycle lanes on surrounding streets. Non-contiguous sidewalks will be constructed throughout all streets within and directly adjacent to project boundaries (except for

existing sidewalk along Broadway) to connect with existing City sidewalks, enhance the pedestrian experience, and meet the standards of the Main Street Streetscape Master Plan along Main Street. The sidewalk and parkway widths along Main Street are sized according to the Main Street Streetscape Master Plan. Should a future bus stop be designated along the project boundary, space will be available for transit stop amenities (such as bench, shelter, trash can) due to the width of landscaped areas provided adjacent to all streets.

Objective LUT-23

Promote the use of non-polluting and renewable alternatives for mobility through a system of bicycle and pedestrian paths and trails that are safe, attractive and convenient forms of transportation.

Policies:

LUT 23.8a Use traffic calming techniques to improve integration between pedestrians, bicyclists and motor vehicles.

LUT 23.8b Ensure that pedestrian routes and sidewalks are integrated into continuous networks.

LUT 23.8c Establish street/sidewalk/trail design guidelines that accommodate a range of users; including access for disabled and bicyclists where feasible.

LUT 23.10 Promote the system of trails envisioned within the Chula Vista Greenbelt.

LUT 23.11 Implement recommendations of the City's Bikeway Master Plan and Greenbelt Master Plan.

LUT 23.14 Require new development projects to provide internal bikeway systems with connections to the citywide bicycle networks.

The Specific Plan infrastructure design includes non-contiguous sidewalks along both sides of Faivre Street, both sides of 7th Avenue (except adjacent to existing offsite apartment building in order to protect existing mature trees and adjacent improvements), Fourth Avenue, and the project frontage along Main Street, consistent with the design parameters of the approved Main Street Streetscape Master Plan. The use of landscape between the street and sidewalk will enhance the pedestrian experience throughout the Specific Plan area. All corner ramps will meet ADA standards. Bicycle lanes are required as part of circulation and access improvements to the site, and the Specific Plan encourages businesses to provide locker room and shower facilities for employees commuting by bike. The Future Park area is within the OVRP and provides opportunities for trail connections to existing OVRP trails. The Specific Plan notes the requirement for park design to use the OVRP Design Standards and Guidelines and the Chula Vista Greenbelt Trail guidelines.

Southwest Planning Area – Main Street District:

The ORBP Specific Plan is located within the Main Street District, which extends between Interstate 5 and Interstate 805, and functions as a commercial-industrial service area. It interfaces with the Otay Town residential neighborhoods north of Main Street and with the Otay River Valley open space to the south. The Main Street

District is the focus of limited industrial uses within western Chula Vista. The General plan notes that the appearance of this heavily traveled thoroughfare has improved over the years due to conformance with design standards that encourage attractive buildings and street frontages. The former mining activities within and adjacent to open space areas have ceased, and the district is envisioned to include a balance between restoration of habitat, new employment and other recreation uses.

The City adopted the Main Street Streetscape Plan which includes more specific goals for the Main Street frontage (discussed later in this Specific Plan). The ORBP Specific Plan will continue to implement the intent for attractive buildings, streetscape design for Main Street, and would extend the pedestrian and landscape elements to other streets in the new business park. The uses allowed by the Specific Plan include a range of industrial, office and support retail for new employment, as well as preservation of existing habitat areas onsite, and designation of a potential community park site as indicated by the General Plan Land Use Element.

Objective LUT-45

Provide for and enhance a strong business district along Main Street that can be balanced between meeting the community's economic needs and establishing a strong open space connection with the nearby neighborhoods.

Policies:

LUT 45.2 Explore opportunities to restore sensitive habitat areas between Broadway and Beyer Way where mining activities currently exist, through state mandated mining reclamation plans.

As of November 2012, the former mine on the project site was determined to be reclaimed with no outstanding reclamation liabilities, per the California Office of Mine Reclamation. A small amount of sensitive habitat was located on the site in the southwest corner, and is being protected by the Specific Plan which prohibits grading or disturbing the area, and creates a buffer between this area and the development. The Specific Plan also provides for the slope area between the floodway and development pad areas to be revegetated with native species to provide a transitional buffer to the parcel to the south and the Otay River Valley.

LUT 45.3 Explore opportunities to provide sensitively designed active recreation parks adjacent to the Otay River Valley to meet local, as well as regional, park needs.

The Specific Plan sets aside a portion of the site for future purchase by the City of Chula Vista to create a community park between Fourth Avenue, Faivre Street and the Otay River Valley.

LUT 45.6 Maintain Main Street primarily as a limited industrial corridor.

The Specific Plan will provide for new business park uses in the Main Street corridor that will accommodate light industrial and office uses, along with supportive commercial uses adjacent to Main Street to serve existing facilities and neighborhoods and provide employment opportunities. The business park uses will generate tax revenue for the City and provide job opportunities for residents.

LUT 45.11 Allow a maximum floor area ratio of 0.5 and low-rise buildings in Limited Industrial designated areas in the Main Street Corridor.

The Specific Plan regulations specify a Floor Area Ratio of 0.5 in Planning Area 1 adjacent to Main Street, as well as Planning Areas 2 and 3. Maximum height allowed by the Specific Plan is limited to 45-feet.

LUT 45.13 The appropriate Specific Plans for the Main Street Corridor shall include design guidelines and standards that address urban development adjacent to the Otay Valley Regional Park.

The Specific Plan site is within the Otay Valley Regional Park (OVRP) boundary. To avoid possible flooding to the developed areas, the grading design shown on the tentative map included with the Otay River Business Park Specific Plan would elevate the building pad above the Otay River floodway, and creates a slope buffer between the building pads and the floodway planted with native species. The Plan also limits uses in Planning Area 4 along the southern boundary of the Specific Plan to open space uses allowed in the Chula Vista Municipal Code for floodways. The OVRP Design Standards and Guidelines are referenced within the SP to guide certain architectural and site plan development within the SP area, including the future park area.

LUT 45.15 Community amenities to be considered for the Main Street District as part of any incentive program should include, but not be limited to those listed in Policy LUT 27.1.

The Specific Plan incorporates several of the community benefits and amenities as indicated in Policy LUT 27.1, such as improvements of all streetscapes on existing streets around the site and new streets within the Specific Plan, creating through-block connections by requiring sidewalks and bike lanes to connect public rights-of-way, and pedestrian path improvements with landscaping adjacent to all sidewalks for a more pleasant pedestrian experience. Additional land is being made available for the City to purchase and develop as a community park. Within the developed area of the Specific Plan, lots are large enough to accommodate larger businesses that could provide greater opportunities for employment by residents.

3.2 Economic Development

Objective ED-1

Provide a diverse economic base for the City of Chula Vista.

Policies:

ED 1.4 Increase the supply of land for non-retail employment through the designation of land to accommodate a regional technology park; a future business park; industrial or business park space; and development of a university campus.

ED 1.6 Promote economic development that fosters job availability, economic revitalization and tax revenues.

The Specific Plan establishes land use areas for a new business park with lots in Planning Areas 2 and 3 sized for development to accommodate larger businesses and employment centers, providing opportunities for a wider variety of employment opportunities. The construction of new industrial and business park uses, with street and infrastructure improvements, will help to revitalize the Main Street corridor.

Objective ED-3

Retain and grow a mix of economically sustainable small and mid-sized industries and businesses.

Policies:

ED 3.5 Provide for ancillary commercial development in business parks, such as banks and restaurants, in order to provide amenities for future tenants.

ED 3.6 Encourage and facilitate a diversity of the type of small businesses established in proximity to one another so as to avoid redundancy of uses within a single block, commercial mall, or neighborhood, but do not preclude clusters of mutually supportive businesses such, as found in a "restaurant row".

Planning Area 1 consists of smaller parcel sizes with the potential for smaller businesses to serve the needs of nearby residents and businesses. A variety of commercial and retail uses are allowed to provide services that complement the new employment uses as well as the existing community. The mix of uses will help to create a functional business park with both large and small facilities and space for ancillary businesses to operate.

3.3 Public Facilities and Services Element

Objective PFS-1

Ensure adequate and reliable water, sewer, and drainage service and facilities.

Policies:

PFS 1.4 For new development, require on-site detention of storm water flows such that, where practical, existing downstream structures will not be overloaded. Slow runoff and maximize on-site infiltration of runoff.

The proposed tentative map includes a bioretention basin to clean and manage storm water runoff created by the backbone infrastructure to be constructed as shown on the tentative map. Management of additional runoff created by construction on individual parcels is required by the Specific Plan to be addressed as part of the parcel development plan.

Objective PFS-2

Increase efficiencies in water use, wastewater generation and its re-use, and handling of storm water runoff throughout the City through use of alternative technologies.

Policies:

PFS 2.2 As part of project construction and design, assure that drainage facilities in new development incorporate stormwater runoff and sediment control, including state-of-the-art technologies, where appropriate.

The proposed drainage system for the tentative map incorporates bioretention basins and storm drains to direct runoff on the developed portion of the site to the existing storm drain outlet, which discharges to the Otay River. Offsite drainage through the site occurs from the area north of the existing alley, and has been accommodated with the proposed storm water design. Additional studies will be required during the development plan/site development phase when parking lots and structures are proposed, but all stormwater runoff and sediment based on the tentative map is included in the stormwater system design.

Objective PFS-14

Provide parks and recreation facilities and programs citywide that are well-maintained; safe; accessible to all residents; and that offer opportunities for personal development, health, and fitness, in addition to recreation.

Policies:

PFS 14.4 Use park dedication; location; site design; and acceptance standards, as provided in the Chula Vista Parks and Recreation Master Plan; the Park Dedication Ordinance; and the Recreation DIF, as may be amended from time to time.

A portion of the Specific Area site is set aside and not developed so that it may remain available for the City to purchase to use as a community park in accordance with the 2005 General Plan that indicates a park in the general vicinity of the Specific Plan site. The OVRP also identifies this area as a potential community park with trail connections to the existing OVRP trails. The SP requires that development of this park use OVRP Design Standards and Guidelines.

3.4 Environmental Element

Objective E1

Conserve Chula Vista's sensitive biological resources.

Policies:

E 1.1 Implement the City of Chula Vista MSCP Subarea Plan.

The Otay River Business Park Specific Plan is located within the Chula Vista MSCP, and within the "Central City Preserve Management Area", but supports no conservation areas or covered projects, according to the biology report associated with the Specific Plan. There is land to the east between the site and Fourth Avenue that is 100% conservation area, with Planning Area 5 Future Park Area situated between developed area and that conservation parcel. There is a small area of sensitive habitat in the southwestern portion and along the southern project boundary of the site that is being preserved with no grading or other disturbing activity. No other sensitive plant species were identified during a survey done for

the biology report. One Species of Special Concern, a Yellow Warbler bird, was seen onsite in the future park area. The site is not directly adjacent to Otay River Valley as there is a parcel adjacent to the south within City of San Diego boundaries that is between the Specific Plan area and the Otay River Valley. An additional buffer is created with the proposed Planning Area 4 along the southern boundary that is within the floodway of the Otay River, and where no permanent development will take place as part of the Specific Plan except as allowed in Chula Vista Municipal Code for floodway areas.

Objective E-2

Protect and improve water quality within surface water bodies and groundwater resources within and downstream of Chula Vista.

Policies:

E 2.5 Encourage and facilitate construction and land development techniques that minimize water quality impacts from urban development.

The mass grading design of the Tentative Map incorporates a stormwater system with bioretention basin improvements that will filter and clean runoff from the site prior to discharging into the Otay River through an existing outlet. Erosion control to reduce sediment prior to individual lot development will be in place through a system of desiltation basins included in the mass grading plan. Additional stormwater treatment via basins or modular wetland systems may be required on individual lots as part of any Development Plan.

Objective E-3

Minimize the impacts of growth and development on water supply resources through the efficient use and conservation of water by residents, businesses, and city government.

Policies:

E 3.1 Promote state-of-the-art water conservation practices in existing and new development, where proven to be safe and environmentally sound.

E 3.2 Promote the use of low water demand landscaping and drought tolerant plant materials in both existing and new development.

E 3.3 Where safe and feasible, promote and facilitate the continued use of recycled water in new developments, and explore opportunities for the use of recycled water in redevelopment projects.

The landscaping design policies call for the use of primarily drought-tolerant plant materials. Landscape standards for the manufactured slope and the erosion control area, and along the landscaped areas and transition near the OVRP are based on the OVRP Landscape Standards. This includes the use of California natives along the manufactured slopes at the southern boundary, floodway and habitat areas, and the future park landscape at park transition areas. The standards for the street frontage abutting the future park area incorporate the OVRP Landscape Standards. Water conservation efforts by individual businesses are encouraged under Chapter 5.5, Sustainable Design Strategies.

3.5 Growth Management Element

Objective GM-2

Provide adequate and sustainable fiscal base.

Policies:

GM 2.1 Achieve and maintain a balance of land uses within the City that assures residential development is complemented by expanded local employment opportunities, retail and commercial services, and recreation and entertainment venues; and that the City-wide mix of land uses provides fiscal balance between those that produce revenues and those that require public expenditures.

In accordance with the General Plan designation of the site for non-residential uses, the Otay River Business Park Specific Plan contributes to a balance of land uses within the City. The Specific Plan allows for a mix of industrial, light industrial, and commercial uses, with the potential for varying sized spaces, to be attractive to range of businesses that can provide a mix of employment opportunities. It also designates currently undeveloped land as a potential future community park site for recreation purposes in the area.

3.6 General Plan Implementation

Objective GPI-2

Provide consistency between the Chula Vista General Plan and subsequent documents, plans, projects, and development.

Policies:

GPI 2.2 Require findings of consistency with the General Plan for all subdivisions, planning approvals and building permits.

Findings of consistency of the Otay River Business Park Specific Plan with the Chula Vista 2005 General Plan have been provided in this chapter.

3.7 Statement of Consistency

The guidelines and standards created by the Otay River Business Park Specific Plan are consistent with the City of Chula Vista General Plan as detailed above.

The specific plan includes text and diagrams which specify:

- (1) the distribution, location, and extent of the uses of land, including open space, within the area covered by the plan on a parcel-specific basis, and implements the generalized boundaries of the General Plan for industrial uses, open space associated with the Otay River, and a future community park site in the area.
- (2) The proposed distribution, location, and extent and intensity of major components of public and private facilities proposed to be located within the area covered by the

plan and needed to support the land uses described in the plan, including circulation, sewage, water, drainage, and other essential facilities.

- (3) Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources within the plan boundaries.
- (4) A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out paragraphs (1), (2), and (3).

CHAPTER 4 - LAND USE & DEVELOPMENT REGULATIONS

4.1 Purpose

The purpose of these regulations is to establish development standards to regulate land uses and development of property within the Otay River Business Park Specific Plan. This Chapter specifies regulations for land use, site planning, development standards, and intensity of land use in order to: 1) accommodate the implementation of the goals of the 2005 Chula Vista General Plan called for in this Specific Plan; 2) address flood hazard constraints at the property; 3) maintain and enhance community character; and 4) protect sensitive natural resources located in the Plan area or in nearby areas.

4.2 Applicability

Unless otherwise specified, the provisions of this Chapter shall be applicable to all property within the geographic boundaries of the Otay River Business Park Specific Plan, and are the controlling provisions of this Specific Plan. The development standards contained in this Chapter supplement or replace those found in the Chula Vista Municipal Code (CVMC). Unless specified in this Chapter with a different standard than the existing CVMC, the provisions of the CVMC, including but not limited to compliance with CVMC 19.58 regarding additional requirements per type of use, shall apply. Where there is a conflict between the provisions of this and other Chapters of the CVMC, the provisions of this Chapter shall prevail. Exceptions to, or relief from, the requirements found within this Section are limited to those permitted with the CVMP Chapter 19.14 pertaining to variances. Compliance with the development standards embodied in the Specific Plan will assure consistency with the purposes of the Specific Plan and meet the objectives stated in the Specific Plan.

4.3 Land Use Plan and Map

The following areas shown on Exhibit 9 Specific Plan Land Use Map are included as part of the Otay River Business Park Specific Plan:

1. Planning Area 1 (Commercial) – Situated adjacent to Main Street, between Fourth Avenue and 7th Avenue at the north edge of the SP area.
2. Planning Area 2 (Transitional-Limited Commercial/Business Park) – Area of property south of the PA-1 (Commercial) and north of Faivre Street between Fourth Avenue and 7th Avenue, and lots adjacent to Broadway to the south of Faivre Street.
3. Planning Area 3 (Business Park) – Remainder of developed lots within the Specific Plan excluding Planning Areas 1 and 2. These include lots north of Faivre Street and west of 7th Avenue, and lots south of Faivre Street that do not border the frontage with Broadway.

4. Planning Area 4 (Floodway and Habitat Area) – Located along the southern boundary of the SP.
5. Planning Area 5 (Future Park Site) – Situated in the eastern portion of the SP, from Fourth Avenue south to Specific Plan boundary.



Legend

Planning Area



**SPECIFIC PLAN
LAND USE MAP**



**OTAY RIVER BUSINESS PARK
SPECIFIC PLAN**

Source:
SanGIS 8-15
Smith Consulting
Architects

**EXHIBIT
9**

4.4 Land Use Matrix

Otay River Business Park Specific Plan Land Use Matrix					
P = Permitted CUP = Permitted with CUP --- = Prohibited	Planning Areas				
	PA-1 Commercial	PA-2 Transitional Area	PA-3 Business Park	PA-4 Floodway and Habitat Area	PA-5 Future Park
Residential					
All forms of residential use.	---	---	---	---	---
Institutional					
Group Assembly	---	---	---	---	---
Out-patient surgery centers, subject to the provisions of CVMC 19.54 and 19.58.	CUP	CUP	CUP	---	---
General Industry					
Manufacture, research, assembly, repair, processing, and packaging of products from raw materials.	---	---	---	---	---
Manufacture, assembly, repair, and packaging of products from construction materials such as stone, clay, glass, lumber, wood, or other similar materials.	P	P	P	---	---
Beer brewing or wineries requiring a Type 23 Alcoholic Beverage Control license.	P	P	P	---	---
Distilling of liquors requiring a Type 23 Alcoholic Beverage Control license.	CUP	CUP	CUP	---	---
Graphics and art equipment, including signage.	P	P	P	---	---
Other accessory uses associated with permitted general industry uses, such as but not limited to administration offices, incidental services, such as restaurants to serve employees when conducted on the premises, and other accessory uses customarily appurtenant to a permitted use.	P	P	P	---	---
Other General Industry uses determined by the Zoning Administrator to be of the same general character as the above permitted uses.	CUP	P	P	---	---

Otay River Business Park Specific Plan Land Use Matrix					
P = Permitted CUP = Permitted with CUP --- = Prohibited	Planning Areas				
	PA-1 Commercial	PA-2 Transitional Area	PA-3 Business Park	PA-4 Floodway and Habitat Area	PA-5 Future Park
Light Industry and Research					
Manufacture, research, assembly, repair, processing, and packaging of products from previously prepared materials.	P	P	P	---	---
Manufacture, processing, and packaging of food products, pharmaceuticals, or similar.	P	P	P	---	---
Research, testing, processing, packaging, and associated light manufacturing of biochemical and chemical materials, electronics, pharmaceuticals, medical equipment, medical and dental labs, and compatible products and materials.	P	P	P	---	---
Businesses engaged in the storage, warehousing, distribution, or wholesale sales of products similar to permitted Light Industry and Research products.	P	P	P	---	---
Businesses engaged in the rental of self-storage units.	---	---	---	---	---
Other accessory uses associated with permitted light industry uses, such as but not limited to administration offices, incidental services, such as restaurants to serve employees when conducted on the premises, and other accessory uses customarily appurtenant to a permitted use.	P	P	P	---	---
Other light industry and research uses determined by the Zoning Administrator to be of the same general character as the above permitted uses.	CUP	CUP	CUP	---	---
Services					
Medical, optical and dental laboratories, not including related products for general sale or distribution except as an accessory use.	P	P	P	---	---

Otay River Business Park Specific Plan Land Use Matrix					
P = Permitted CUP = Permitted with CUP --- = Prohibited	Planning Areas				
	PA-1 Commercial	PA-2 Transitional Area	PA-3 Business Park	PA-4 Floodway and Habitat Area	PA-5 Future Park
Medical, optical, and dental offices and clinics.	P	P	---	---	---
Veterinarian offices or animal hospitals, subject to the provisions of CVMC 19.58.050.	P	P	P	---	---
Veterinarian offices or animal hospitals with outdoor facilities for large animals, subject to the provisions of CVMC 19.58.050.	CUP	CUP	CUP	---	---
Administrative, business or professional offices.	P	P	P	---	---
Financial offices such as banks and similar, real estate services, and other financial services.	P	P	---	---	---
Blueprinting, copy services, photographic services, printing, publishing.	P	P	P	---	---
Specialized studios for arts and crafts, photography, music, dance, art galleries, and similar, in accordance with the provisions of CVMC 19.58.220.	P	P	---	---	---
Personal care services, such as barbershop and beauty salons and similar.	P	---	---	---	---
Automobile service stations, subject to the provisions of CVMC 19.58.280, and car washes subject to the provisions of CVMC 19.58.060.	CUP	CUP	CUP	---	---
Automobile minor maintenance and repair.	CUP	P	---	---	---
Automobile major repair centers.	---	CUP	P		
Health or athletic clubs, sports and health classes and clinics, and other businesses of the same character.	P	P	---	---	---
Commercial recreation facilities such as bowling alleys and skating rinks, subject to CVMC 19.58.040.	---	---	---	---	---
Commercial recreation facilities, outdoor.	---	---	---	---	---
Social or fraternal organizations subject to the provisions of CVMC 19.58.100.	---	---	---	---	---

Otay River Business Park Specific Plan Land Use Matrix					
P = Permitted CUP = Permitted with CUP --- = Prohibited	Planning Areas				
	PA-1 Commercial	PA-2 Transitional Area	PA-3 Business Park	PA-4 Floodway and Habitat Area	PA-5 Future Park
Hotels, motels, and similar uses.	---	---	---	---	---
Other service and office uses determined by the Zoning Administrator to be of the same general character as the above permitted uses.	CUP	CUP	CUP	---	---
Commercial Retail					
Retail commercial businesses.	P	---	---	---	---
Coin-operated laundry, with maximum capacity washing units of 20 pounds and comparable drying equipment, and dry-cleaners.	P	---	---	---	---
Building material retail sales and equipment rental.	P	P	P	---	---
Plant nurseries and the sale of related items.	P	P	P	---	---
Other commercial uses determined by the Zoning Administrator to be of the same general character as the above permitted uses.	CUP	CUP	CUP	---	---
Commercial, Food Services					
Grocery, general or specialty (such as bakery products, produce, meat, bread), with or without associated delicatessen.	P	---	---	---	---
Restaurants, delicatessens, coffee shops, and similar uses with associated seating.	P	P	---	---	---
Fast food or similar uses with drive-through lanes. Limited to 3 drive-through establishments within the Specific Plan.	CUP	---	---	---	---
Drive-in restaurants where food is ordered from and consumed in the parked car on the premises.	CUP	---	---	---	---
Tasting Room, only as an accessory use to on-site brewery or distillery, or social or fraternal organization.	P	P	P	---	---
Liquor store (package, off-sale only), in accordance with the procedures of CVMC 19.58.340.	CUP	---	---	---	---

Otay River Business Park Specific Plan Land Use Matrix					
P = Permitted CUP = Permitted with CUP --- = Prohibited	Planning Areas				
	PA-1 Commercial	PA-2 Transitional Area	PA-3 Business Park	PA-4 Floodway and Habitat Area	PA-5 Future Park
Bars, tasting rooms not associated with on-site brewery or distillery, and cocktail lounges.	CUP	CUP	---	---	---
Restaurants with dance floors or areas designated for live entertainment, and night clubs.	P	CUP	---	---	---
Other food service uses determined by the Zoning Administrator to be of the same general character as the above permitted uses.	CUP	CUP	---	---	---
Other					
Open Space without permanent structures, subject to the provisions of CVMC 19.50 Flood Zone Regulations.	---	---	---	P	P
Public park with related structures such as restrooms, play areas, athletic fields, courts and similar recreational uses with associated seating, parking, lighting, and landscaping.	---	---	---	---	P

4.5 Accessory Uses and Structures

1. Incidental services such as prescription pharmacies and retail sales of products produced or manufactured on the site, when conducted and entered from within the building, provided there is no display or advertising visible from the public street.
2. Administrative, executive, and financial offices, and incidental services like restaurants to serve employees, when conducted on premises.
3. Other uses and buildings customarily appurtenant to permitted uses.
4. All accessory uses and structures must comply with applicable provisions of CVMC 19.58.020.

4.6 Floodway and Habitat Area (Planning Area 4)

Portions of the Specific Plan property are located within the floodway of the Otay River, with a small amount of Southern Willow scrub in the southwest corner of the Site. General grading and development are not permitted within the floodway,

except as allowed in CVMC 19.50.040 and subject to all regulations by FEMA and in CVMP 19.50 and 14.18. Should there be an inconsistency between these regulations, FEMA guidelines and CVMC 14.18 shall be the controlling regulations. Remedial grading shall be permitted so long as the disturbed area is returned to the original grade and certified by a registered civil engineer.

4.7 Future Park Area (Planning Area 5)

A portion of the Specific Plan property is identified as a location for a future Community Park by the City of Chula Vista General Plan Public Facilities and Services Element and the Otay Valley Regional Park (OVRP) Concept Plan. Approximately 13.9 acres are set aside in this Specific Plan to maintain the option for this area to serve as a future Community Park site available for purchase by the City of Chula Vista. The final design of the park, including trail connections to the OVRP, will be determined by the City, after the City has agreed to purchase the park area, in collaboration with the community and shall use the most current OVRP Design Standards and Guidelines.

4.8 Biological Open Space Easement

There are two areas within the Specific Plan with an existing biological habitat that are to remain undisturbed: one located in the southwest corner of the SP and one along the southern edge within the floodway (both within Lot B). These environmentally sensitive areas are to be protected from any grading or development, and a biological open space easement shall be established over the areas prior to site development. The specific easement boundaries are identified on the proposed TM.

4.9 Interim Uses

Existing uses, buildings, and/or structures that become nonconforming due to adoption of the Otay River Business Park Specific Plan shall not be allowed to continue in operation, and shall be abated no later than six months from the date the Specific Plan becomes effective. Any future use of property within the boundaries of the Specific Plan shall conform to the provisions of this Specific Plan.

4.10 Development Regulations

A. Lot Size and Dimensions

Minimum lot size: 10,000 square feet

Minimum lot dimensions: Width 100 feet

Depth 100 feet

B. Setbacks

Minimum setback criteria is provided in the following table. The Building Setback applies to all structures within the Specific Plan site, and the Parking Setback applies to parking lot areas within the Specific Plan site.

Setback Location	Building Setback (min)	Parking Setback (min)
Main Street	10 feet	10 feet
Fourth Avenue	10 feet	10 feet
Faivre Street	10 feet	10 feet
7 th Avenue	10 feet	10 feet
Broadway	10 feet	10 feet
Alley	5 feet	0 feet
Side (Interior lot lines)	5 feet	0 feet
Future Park Area Boundary	10 feet	5 feet
Rear	5 feet	5 feet

C. Height

The maximum building height shall be forty-five feet (45'-0") above the finished grade.

Height limitations shall not apply to the following:

1. Flag poles, radio towers, masts and aerials, or to parapet walls extending not more than 4 feet above the height of the building.
2. Elevator and stair penthouses provided no lineal dimension of such structure exceeds 50% of the corresponding street line frontage.
3. Other exceptions not noted but provided for in CVMC 19.16.040.

D. Lot Coverage

The maximum lot coverage shall be fifty percent (50%) in Planning Areas 1, 2 and 3. Lot coverage shall be calculated as defined in CVMC 19.04.060.

E. Floor Area Ratio

The maximum Floor Area Ratio (FAR) shall be 0.5 in Planning Area 1 and 1.0 in Planning Areas 2 and 3.

Floor Area Ratio (FAR) is a measure of the bulk of buildings on a lot or site. FAR is calculated by dividing the gross floor area of all buildings on a lot or site by the lot or site area. Gross floor area includes the total enclosed area of all floors of a building measured from the exterior walls including halls, stairways, elevator shafts at each floor level, service and mechanical equipment rooms, balconies, recreation rooms, and attics having a height of more than seven feet

but excluding area used exclusively for vehicle parking or loading. For example, a two-story building occupying one-half of a site has an FAR of 1.0.

F. Parking Standards

Size of Parking Spaces and Drive Aisles

Standard parking spaces shall be a minimum of 9 feet wide by 19 feet deep, with 24-foot two-way drive aisles or 15-foot one-way drive aisles. Compact parking spaces shall be a minimum of 8 feet wide by 15 feet deep, and may be provided for up to 10 percent of the total spaces required. A minimum of 10-foot high vertical clearance on drive aisles shall be provided for emergency vehicle access.

Accessible Spaces

All required accessible (disabled) parking spaces shall be constructed in a manner required by the most current edition(s) of the State of California Uniform Building Code (Title 24) as amended.

Number of Spaces Required

The required number of parking spaces shall be based upon the cumulative total required for uses within the project as specified in CVMC 19.62.050, which may be amended. Refer to the most current parking table adopted by the City of Chula Vista Planning Commission for relevant additional requirements. For any uses not listed, the City may require a parking study to determine the appropriate number of parking spaces.

For ease of reference, the current parking requirements for typical uses anticipated within the business park are listed below:

LAND USE	GENERAL STANDARD (refer to CVMC 19.62.050 for details)
Office	1 per 300 square feet
Medical/Dental Office	1 per 200 square feet
Manufacturing	1 per 800 square feet
Wholesale and Warehouse	1 per 1,000 square feet
Retail Stores/Financial Institutions	1 per 200 square feet
Restaurants	1 per 2.5 fixed seats
Drive Through Restaurants	15 spaces minimum plus queue for 5 cars
Furniture/Appliance Store	1 per 600 square feet

When the calculation of the requirements for parking results in a fractional portion of a parking space, a fraction of one-half or greater shall be construed as one required space, and a remaining fraction of less than one-half shall be disregarded from the calculation of required spaces.

Shared Parking

Shared parking will be allowed between Planning Areas 1 and 2, and among uses of Planning Areas 2 and 3 that are adjacent to each other in the Specific

Plan. If the required number of parking spaces cannot be provided within a particular parcel of the Specific Plan, a shared parking plan may be allowed with the following conditions:

1. Shared parking shall be for uses located exclusively within the Otay River Business Park Specific Plan and must be located within 500 feet of the individual parcel to be served by the shared parking plan.
2. A Shared Parking Analysis is required demonstrating the demand (number of parking spaces needed) for each use that will participate in the Shared Parking Program throughout a typical day. The hour in which the highest number of spaces is needed (peak parking demand) for the proposed development determines the minimum number of required off-street spaces for that proposed use. The shared parking analysis shall be reviewed and approved by the Director of Development Services and the City Engineer.
3. Shared parking shall be enforced through a shared parking agreement in a form approved by the City of Chula Vista, and maintained in perpetuity through an agreement between parcel owners.
4. Adequate signage shall be provided for tenants indicating the availability of alternate parking areas.
5. Modification to the buildings or uses within the Specific Plan area are located or changes in tenant occupancy requires review by the Development Services Director.

G. Design Review

1. Development in the Otay River Business Park shall be subject to compliance with the applicable standards of review and review processes contained in the CVMC Chapter 19.14, the Chula Vista design review process, and any amendments thereto.
2. In addition to compliance with the design review process, development in the Otay River Business Park shall be in compliance with the Design Guidelines in Chapter 5 of this Specific Plan.

CHAPTER 5 - DESIGN GUIDELINES

5.1 Purpose and Intent

The following guidelines and standards address site planning and design elements applicable to the overall Specific Plan property.

5.2 Site Design Guidelines

A. Circulation and Site Access

Requirements for site access are provided in this section. Frontage Improvement design guidelines are provided in Landscape Design Guidelines, Section 5.4 (B).

1. A new public Faivre Street south of and parallel to Main Street shall be added to create a connection between Broadway and Fourth Avenue and provide access to individual parcels on the site.
2. Multiple driveways on opposite sides of a street should align directly across from each other when feasible, or with an offset acceptable to the City of Chula Vista engineering staff.
3. Driveway dimensions shall comply with City Standards current at the time a Development Plan is submitted.
4. Site design for buildings/uses requiring large semi-truck access shall provide adequate circulation for truck turning and maneuvering throughout the lot, such that any large truck may enter and exit from the public street, and circulate around any structure needing truck access on any parcel without backing up. Shared/joint access routes between parcels and buildings may be used to provide for such circulation, and any such joint access shall be maintained in perpetuity through an agreement between parcel owners.
5. Transit access is available near the Specific Plan site via routes that have stops at Broadway and Main Street, and at Fourth Avenue and Main Street. No current bus routes are directly adjacent to the Specific Plan site. Should bus routes be modified in the future, the parkway design could accommodate bus stops with benches or waiting areas for riders.

Traffic Demand Management (TDM) programs are developed, typically on a regional or large project basis, to reduce vehicle trips, single occupancy vehicles, and air emissions. A TDM program through the City of Chula Vista or the ORBPSP has not been developed, but some TDM program measures have been included in the Tentative Map, and are encouraged through the Specific

Plan Sustainable Design Strategies located in Section 5.5 of this Chapter. Examples include:

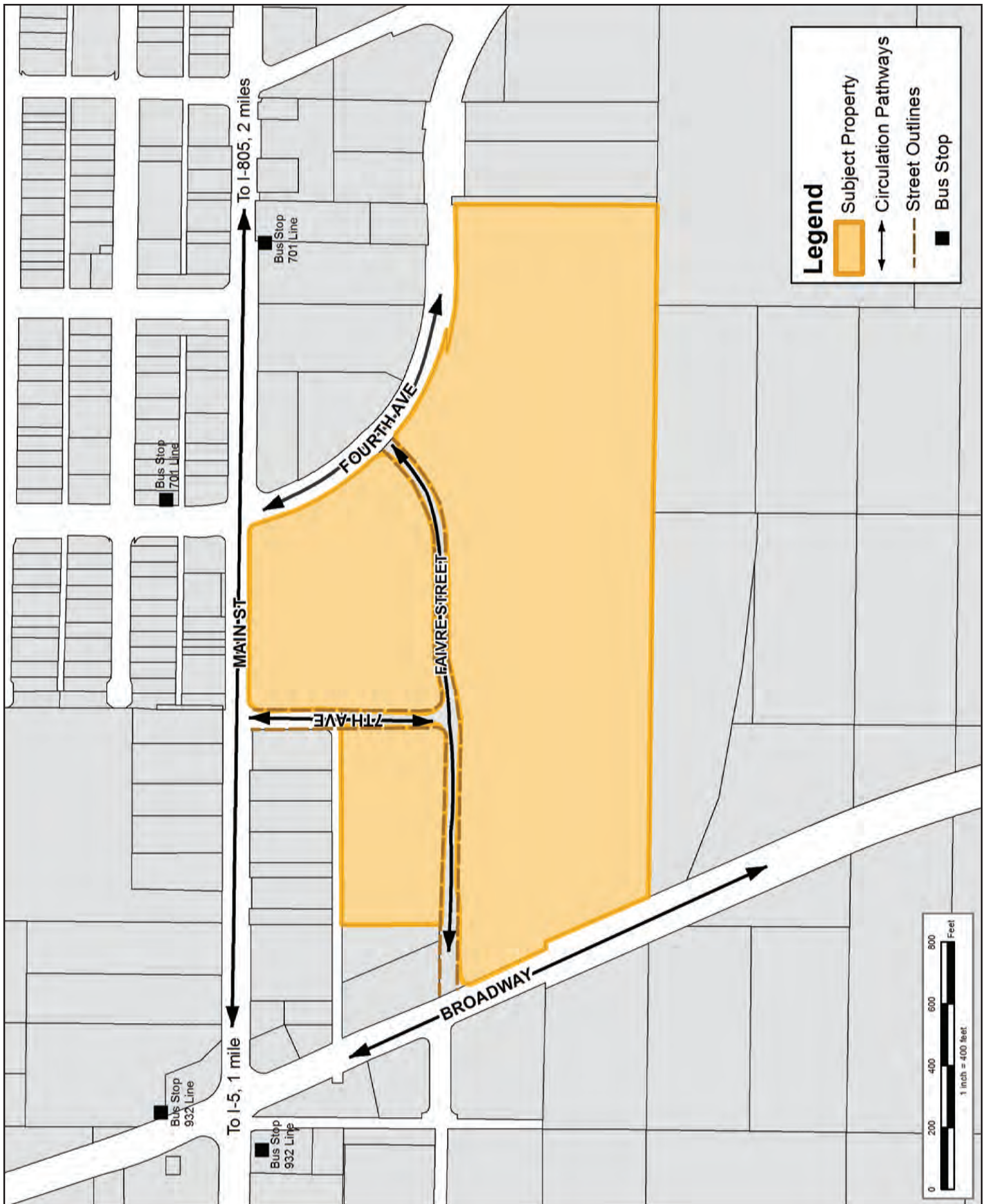
- Bike lanes adjacent to all public roads through and directly adjacent to the Specific Plan where possible,
- Encouraging development plans to include bike parking, and changing rooms and showers for bike commuters, in all buildings.
- Encouraging electric vehicle charging stations throughout the Specific Plan site.
- Providing parkways that can be used for waiting areas should new transit stops be located directly adjacent to the Specific Plan site.

Should a TDM program be established city-wide or region-wide, the ORBP will be available to participate, including in programs for paid parking and shuttle service to accommodate nearby transit services.

Exhibit 10 Circulation Map

B. Parking and Loading

1. All parking shall be provided onsite within the lots of the Specific Plan with no street parking on existing or new public streets.
2. All parking areas shall be screened from view of public streets. Where possible, such screening should be provided by use of a landscaped berm or hedges, at least 3.5 feet high. Alternate screening with masonry walls or fencing may be used and shall not be less than 3.5-feet or more than 6-feet in height, and shall be located beyond the setback.
3. Truck loading areas should be located and designed for minimum visibility from public streets. Placement in front of structures or where highly visible from public streets is discouraged. Where necessary, walls shall be provided to screen truck loading areas from view. Such walls may be extensions of the building walls, wing walls or freestanding walls, and enhanced with landscape planting for aesthetic purposes and to reduce graffiti. The use of screen walls should also be designed to provide noise attenuation if necessary to protect adjacent land uses.



Legend

- Subject Property
- Circulation Pathways
- Street Outlines
- Bus Stop



CIRCULATION MAP



**OTAY RIVER BUSINESS PARK
SPECIFIC PLAN**

Source: SanGIS 8-15

5.3 Architectural Design Guidelines

A. Architectural Character

The Otay River Business Park is situated along Main Street, a primary road servicing a commercial and industrial area of Chula Vista. This Plan provides an opportunity to improve a site, which is largely vacant currently and had been occupied by several heavy industrial operations, and increase economic vitality in the area. The design guidelines in the Otay River Business Park Specific Plan are intended to permit flexibility in the architectural styles of the Specific Plan, while allowing Chula Vista to ensure visual improvement along an important thoroughfare.

The Main Street corridor currently does not have a defined architectural style. Therefore, the focus of the architectural character within the SP should be on creating a high-quality development in which design styles of the various land uses of the Specific Plan coordinate to create a unifying and harmonious overall development. The coordinating elements could include building form, color, material, rooflines, and landscaping. The use of standardized corporate architectural styles is acceptable provided the design complies with the Specific Plan guidelines.

The business park also abuts the Otay River Valley. The OVRP Standards and Guidelines include requirements to consider compatibility and buffering where appropriate to blend new development with the natural setting and character of the OVRP. Additional design standards and guidelines are provided for structures within the Regional Park focused on recreation centers, comfort stations, and picnic shelters. The architectural theme for the OVRP is "Californio" ranchos of the early 1800's, a style appropriate for the open space character of the future park.

B. Building Massing and Scale

1. Building facades and rooflines shall provide a variety of architectural elements but create harmonious aesthetics between Planning Areas 1 and 2 of the Specific Plan.
2. Heights and wall planes should be staggered to create shadows that break up large expanses of facades.
3. Use varied roof forms, mass, shape, or materials to create variations in building facades.
4. Design buildings that have a street facade which is complementary in scale and massing to its surrounding.
5. Provide a sense of pedestrian scale at the ground level of buildings of Planning Area 1 and Planning Area 2 along Main Street and Fourth

Avenue through appropriate use of building materials, and architectural details.

6. Building entries should be readily identifiable using recesses, columns, or other architectural details to emphasize primary entrances.
7. Building elevations facing the southern slope/Otay River Valley should be evaluated for visibility from OVRP trails. If visible, elevations should be articulated to provide interest, per the OVRP Design Standards and Guidelines, Section 5.3.3, or screened with vegetation to soften views from OVRP trails.

C. Exterior Materials and Finishes

1. Use building materials that are of high quality, durable, require low maintenance, and complement the design of the building.
2. Use a combination of materials, textures, and colors to help articulate the building design to achieve greater variety, visual interest, and richness in the architectural character. Material and finish changes should occur at logical break points. Highly reflective surfaces and large expanses of exposed single-color precision block walls should be avoided.
3. A limited color palette shall be incorporated into the design, with color differentiation used within the overall project to reduce monotony, and avoid repetitiveness within the building facades. Building colors should be appropriate for the architectural style.
4. Accessory structure design shall be compatible with the architecture of the primary structure.

D. Roofs and Roof-Mounted Equipment

1. Design roofs (form, style, and pitch) to further enhance and articulate the architectural vocabulary used in the facades and to be compatible with the style and character of each building.
2. Roof materials should be appropriate to the architectural style of the buildings.
3. Rooflines should be accentuated with appropriate projections and overhangs.
4. All roof appurtenances including, but not limited to, air conditioning units and mechanical equipment shall be shielded and architecturally screened from view from surrounding properties and public rights-of-way. Including trails within the OVRP, using full parapets, low parapets plus screening, sloping roofs, or other means of screening that

coordinate with the architectural style of the building. Solar panels should be integrated into the roof design.

E. Trash Enclosures and Other Screening

1. Trash receptacles, adequate areas for collecting and loading recyclable materials, and storage areas shall be provided within the main structures or in separate free-standing structures. They should be placed at or near the rear of the main structures that they serve and near loading areas or drive aisles when feasible, so long as circulation is not blocked during loading operations. Trash enclosures should be located where possible such that trash trucks can service them without having to reverse out of the service area.
2. All trash enclosures, and utility and mechanical equipment shall be screened per CVMC 19.58.340, and enclosures that could be visible from a public street or walkway shall be further screened with landscaping.
3. Policies of the City of Chula Vista's Recycling and Solid Waste Planning Manual shall be incorporated in site design and followed by individual businesses with the Specific Plan area.

F. Exterior Lighting

1. Exterior lighting shall be provided to illuminate outdoor areas including parking areas, loading areas, pedestrian pathways and building entrances.
2. All exterior lighting should be designed and incorporated into the building schemes to provide illumination appropriate for the level of activity and to avoid glare and spillover.
3. Lighting fixture types, quantities, and locations should be carefully considered to complement the architecture and ensure consistency with the lighting policies of the City of Chula Vista.
4. All fixture illumination shall be directed downward to minimize the spread of the beam, and all lighting standards should be hooded and designed to prevent light spillover. Exterior lights shall be shielded from intrusion into the future park area and to the areas to the south of the business park southern slope (OVRP).

5.4 Landscape Design Guidelines

A. Landscape Objectives

The landscape design guidelines establish general requirements for the various landscape design components of the Otay River Business Park and are intended

to establish a cohesive landscape design theme that contributes to a project identity. They are consistent with current City of Chula Vista Standards, and allow for flexibility in application and execution. The landscape theme uses appropriate native and drought tolerant plant materials that provide a unifying concept that is consistent with the surrounding community.

General Guidelines and Requirements include:

1. The Site landscape should use predominantly drought-tolerant plants, native and/or non-native, that require minimal irrigation and maintenance.
2. Landscape species selection and installation near sidewalks, pathways and paved areas, including streets, shall be based on preventing unreasonable damage to paving and pathways. Tree species with invasive root systems should be discouraged and oriented away from utility infrastructure. All street trees and trees planted within 10 feet of buildings or hardscape shall incorporate a root deflection device at the time of installation.
3. Landscaping should create a focus on entrances to parking lots and entrances to buildings to improve wayfinding for visitors, but be designed and maintained to eliminate spaces hidden from public view that could shield criminal activity.
4. Outdoor eating areas for employees and visitors are encouraged to be located where there are views of the future park area or along the southern boundary overlooking the Otay River Valley.
5. All public areas and rights-of-way shall have an automatic irrigation system to maximize irrigation efficiency and eliminate overspray on the adjacent hardscape surfaces.
6. The project landscaping should be compatible with the adjacent river valley to create a visually harmonious environment, with appropriate visual separation between the adjacent future park activity areas and the Business Park.
7. No less than 15% of each individual parcel shall be landscaped.
8. All landscape plans shall comply with CVMC Chapter 20.12 regarding Landscape Water Conservation.
9. Landscaping shall not impede the function of any biofiltration basins.

B. Frontage Improvements

The landscape design for the project is established for the public street rights-of-way by means of the standards listed below and in Chapter 5 of this Specific Plan, and implements provisions of the Main Street Streetscape Master Plan.

The street frontage along public streets directly adjacent to the Site are of particular importance as they establish the first impression of the Otay River Business Park Specific Plan area and are highly visible to tenants of the business park, the local community, and visitors to Chula Vista. Street frontages on interior streets continue the visual theme throughout the Business Park. Sidewalks are interconnected to each other and to existing streets to improve pedestrian access and experience throughout the Business Park, and the separation of pedestrian and vehicular movement through landscaped parkways increases safety for pedestrians.

Main Street

1. Themes and materials for this frontage have been chosen to be consistent with the Main Street Streetscape Master Plan. The agricultural economy of Chula Vista led to the incorporation of the City, and continued to be an important part of the Chula Vista economy. The ORBP site is within the section of Main Street identified to have the agriculture/rancho district theme in Section 5.5 of the Streetscape Master Plan reflecting this importance.
2. Vehicular access to Planning Area 1 of the Site will be from Main Street, therefore the landscaping along this street frontage should provide visual interest and way-finding for visitors coming by car or on foot.
3. A non-contiguous sidewalk and 7.25-foot parkway, per the Main Street Streetscape Master plan, along with new curb and gutter shall be provided along the frontage, and suitable landscaping provided in the parkway in compliance with the approved Main Street Streetscape Master Plan. Between the sidewalk and parking areas, shrubs and trees should be incorporated to screen parking and mark driveways using plant materials consistent with the Master Plan to extend the visual character of the streetscape onto the Specific Plan site.
4. The agricultural history of this site, as noted in the Main Street Streetscape Master Plan, should be reflected in a portion of the landscape design, such as through rows of planting to suggest row crops either along Main Street or along Fourth Avenue north of Faivre Street. A plaque or interpretive signage should also be included to recognize the former strawberry fields cultivated on the Site.
5. Landscaping elements shall not impede travel in the bicycle lane.

Exhibit 11 Cross Section for Main Street

Fourth Avenue

1. A non-contiguous sidewalk and 6-foot parkway with new curb and gutter is required along the frontage from Main Street to the Specific Plan boundary. Landscaping should be located in the parkway to mark driveways and Faivre Street.
2. As addressed in the Main Street section above, the agricultural history of this site should be reflected in a portion of the landscape design along Fourth Avenue north of Faivre Street if not provided on Main Street. A plaque or interpretive signage should also be included to recognize the strawberry fields on the site prior to the Specific Plan.
3. Landscaping elements shall not impede travel in the bicycle lane.

Exhibit 12 Cross Section for Fourth Avenue

7th Avenue

1. A non-contiguous sidewalk and 7.5-foot parkway with new curb and gutter shall be provided along both sides from Main Street to Faivre Street, except in the section directly adjacent to the apartment parcel where sidewalk shall be contiguous with curb and gutter due to existing limitations. Suitable landscaping to harmonize with Main Street plantings should be located in the parkway, incorporating shrub and tree (where feasible) plantings to screen parking and mark driveways.
2. Landscaping elements shall not impede travel in the bicycle lane.

Exhibit 13 Cross Section for 7th Avenue

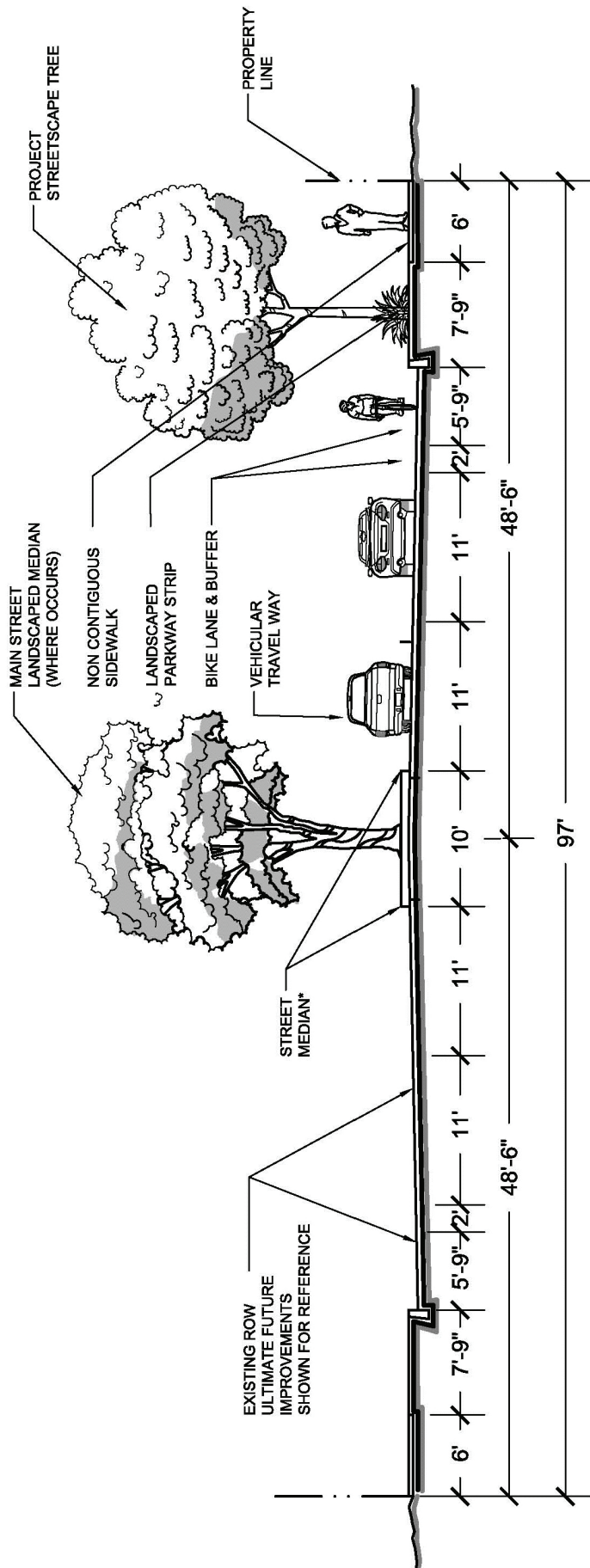
Faivre Street

1. A non-contiguous sidewalk and 6-foot parkway with new curb and gutter shall be provided along both sides of the street between Broadway and Fourth Avenue. Suitable landscaping to harmonize with Main Street plantings should be located in the parkway, incorporating shrub and tree plantings to screen parking and mark driveways.
2. Landscaping elements shall not impede travel on the bicycle lane.

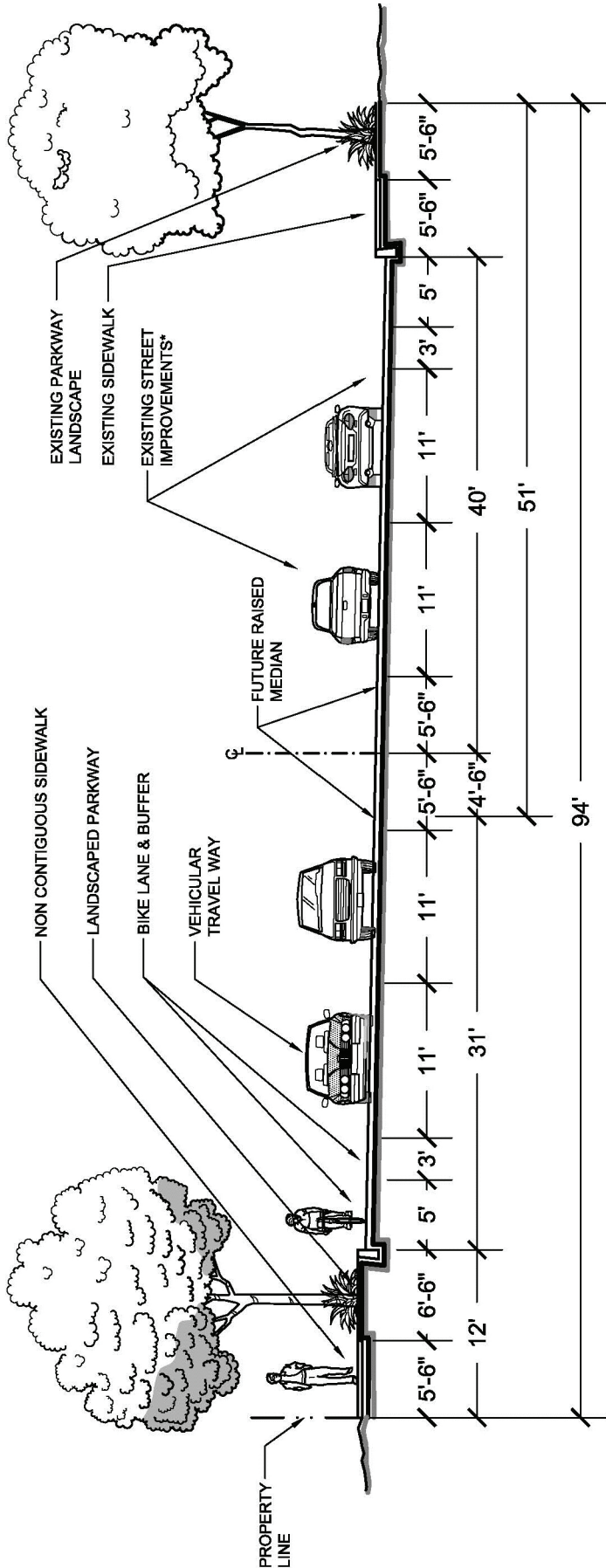
Exhibit 14 Cross Section for Faivre Street

Broadway

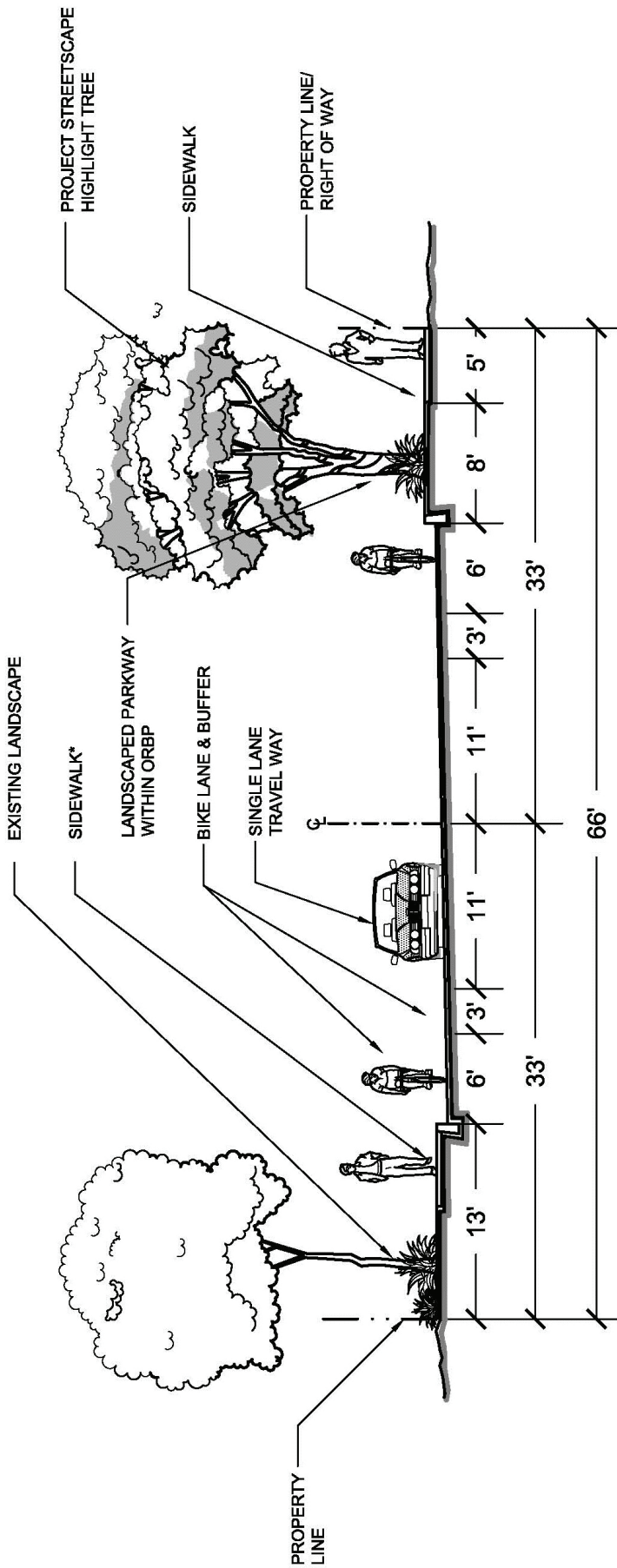
1. An 8-foot sidewalk contiguous with the curb and gutter is existing along Broadway. No additional parkway space or dedications are required. Future development plans should provide landscaping outside of the right-of-way, located between the sidewalk and parking areas and incorporating plantings to mark entries to the Specific Plan site.



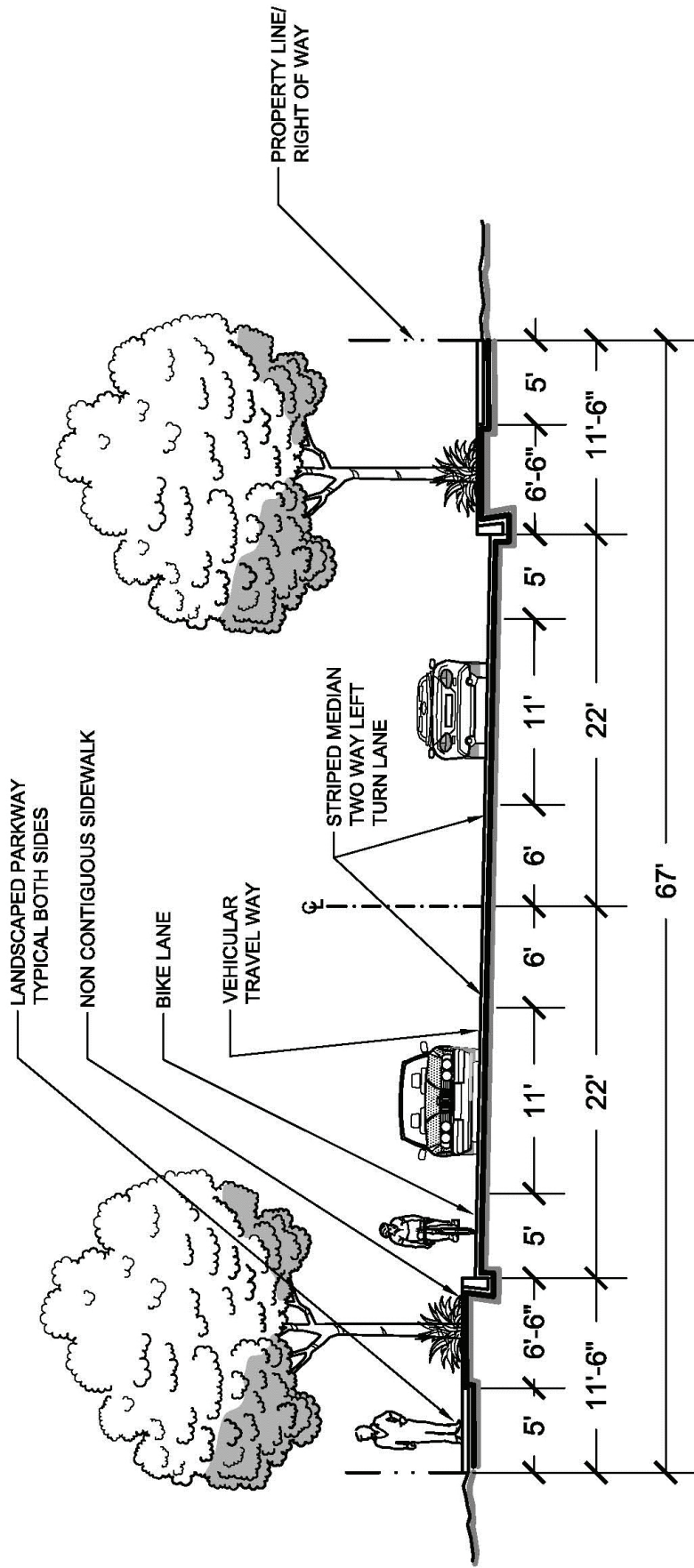
* MAIN STREET MEDIAN TO CONSIST OF RAISED LANDSCAPED MEDIAN OR PAVEMENT STRIPING PER MAIN STREET MASTER PLAN.



* ULTIMATE LANE STRIPING SHOWN FOR REFERENCE



*CONTIGUOUS ALIGNMENT AT EXISTING APARTMENT COMPLEX.
 NON CONTIGUOUS ALIGNMENT WITHIN OTAY RIVER BUSINESS PARK



C. Entries

Project entries from the major streets provide an opportunity to identify and mark arrival to the Business Park. The landscaping at the entries will be a component of the design theme for the Business Park, and will set the tone for landscape design on the individual parcels or projects in the Business Park with the following guidelines:

1. Primary and Secondary Entries should be identified by the use of monument signage and a hierarchy of landscape materials.
2. The agricultural history should be reflected in monument design, along with the layout of the planting to resemble row crops that were formerly on the site, at the entries off of Fourth Avenue and/or Main Street.

D. Otay River Valley Interface

The southern portion of the Specific Plan is located within the floodplain of the Otay River. The planned grading design will raise the development portions of the SP above the 100-year floodplain of the Otay River, resulting in a slope along the southern edge of Planning Areas 2 and 3. The area of the Specific Plan south of the slope is in the floodway, and will be enhanced to create a transition between the developed area and the Otay River Valley. This transition area has been heavily disturbed except for a small area of sensitive biological habitat near the southwest corner of the Specific Plan. These areas will be treated as follows:

Southern Boundary Manufactured Slope

A slope created near the southern edge of the project boundary raises the project site elevation above the Otay River floodplain. The sloped portion of the lots facing the river will create a transition between the developed site and the Otay River Valley. This will be treated as follows:

1. Appropriate native plant material shall be utilized to enhance the area. The landscape palette for this slope is based on the Otay Valley Regional Park Design Standards and Guidelines, especially Section 4.3 Landscaping in Transition Areas for suggested tree species and Section 4.6 Landscaping Standards for All Sites.
2. A permanent irrigation system shall be provided in this area to allow for maintenance and replanting in the future as needed.
3. If any rip rap is needed, concrete rip-rap that is plain and left in the natural color is discouraged. Any rip-rap should be colored or designed in a way to blend with the native vegetation of the slope.

4. Maintenance shall be provided in perpetuity by the parcel owner, or successor in interest, a Business Owner's Association for the Business Park, or other entity.

Floodway

Below the slope on the southern edge of the project boundary is the Otay River floodway. This is a transitional area to the Otay River Valley and will be treated as follows:

1. Any areas south of the slope that need to be disturbed in construction or remedial grading are to be hydroseeded with native species for erosion control. A temporary irrigation system shall be provided in this area during the native plant establishment period, and removed when planting is established, as required in the Otay Valley Regional Park Design Standards and Guidelines, Section 4.4 Landscaping in Natural Areas.
2. Maintenance of the Floodway Lot shall be provided in perpetuity by the property owner, or successor in interest, a Business Owner's Association for the Business Park, or other entity. Alternate maintenance arrangements may be provided if agreed to by the City of Chula Vista.

Habitat Area

A small amount of sensitive habitat is in the southwest corner of the project site. This area will be treated as follows:

1. The habitat area is to be placed in a Biological Open Space Easement.
2. The habitat area shall be protected and preserved, and will not require any supplemental planting unless otherwise determined and directed by conditions placed upon the project by the appropriate resource agencies.

E. Bioretention

Bioretention basins are located per the tentative map to handle all water runoff created by the Specific Plan area tentative map. Additional basins to control additional runoff created by development of individual parcels may be created in development plans for those parcels.

1. Maintenance shall be provided in perpetuity by the property owner, or successor in interest, a Business Owner's Association for the Business Park, or other entity.

F. Future Park Area

A future Community Park site is designated in the vicinity of the Specific Plan area by the Chula Vista General Plan discussion of the Main Street District. This

area will be retained in its current condition and designated as a potential future park space available for purchase by the City of Chula Vista.

1. A Wetland Area is located on the southern end of the flood control channel as delineated on the tentative map. Any impact to this area during development of the Community Park shall be mitigated per City, State and Federal agency requirements as described in the Biological Assessment Report.
2. Former retention basins used during mining operations are currently considered Freshwater Marsh and therefore a biological resource. Any permanent impact to these areas during development of the Community Park shall be mitigated per City, State and Federal agency requirements as described in the Biological Assessment Report. Temporary impacts to these areas during any phase of development shall be revegetated with plant species appropriate to the Wetland but will not require additional mitigation.
3. Permanent landscaping is required only along the western edge of this area at the slope abutting the Business Park development, and along the parkway frontage of Faivre Street and Fourth Avenue. These areas shall be native plantings to support the OVRP design standards.
4. Any areas within the future park area adjacent to the slope or the proposed sidewalks that need to be disturbed in construction or remedial grading are to be hydroseeded with native species for erosion control. A temporary irrigation system shall be provided in disturbed areas during the native plant establishment period, and removed when planting is established or further landscaped by the City of Chula Vista in conjunction with park development.
5. Landscape design within the future park area shall refer to the OVRP Design Standards and Guidelines, especially Section 4.2 Landscaping in Urban Areas and Section 4.6 Landscaping Standards for All Sites, and the Chula Vista Greenbelt Trail design guidelines.
6. Erosion and silt control is required until the site is purchased and developed by the City.

G. Planting

Landscape design concepts for the Business Park are established to create a project identity through the creative use and placement of the plant materials in the streetscape and project entry landscape design. The agricultural economy of Chula Vista led to the incorporation of the City, and continued to be an important part of the City's economy. This importance influenced the agriculture/rancho-themed portion of the Main Street Streetscape Master Plan. The proposed street trees incorporate some of the tree suggestions of this Plan in the landscape, along with other coordinated plantings. They are a mixture of

evergreen and deciduous trees that provide interest by the way of form, texture and color. The ground plane provides color and interest for pedestrians, and can be arranged in a way to suggest the former row crops that existed on a portion of the Specific Plan site.

Landscape design concepts adjacent to the Future Park area have been established to implement the Design and Development Standards of the Otay Valley Regional Park. Street trees and corresponding planting selections adjacent to this lot rely on native species to emphasize the connection of this lot to the Otay River Valley.

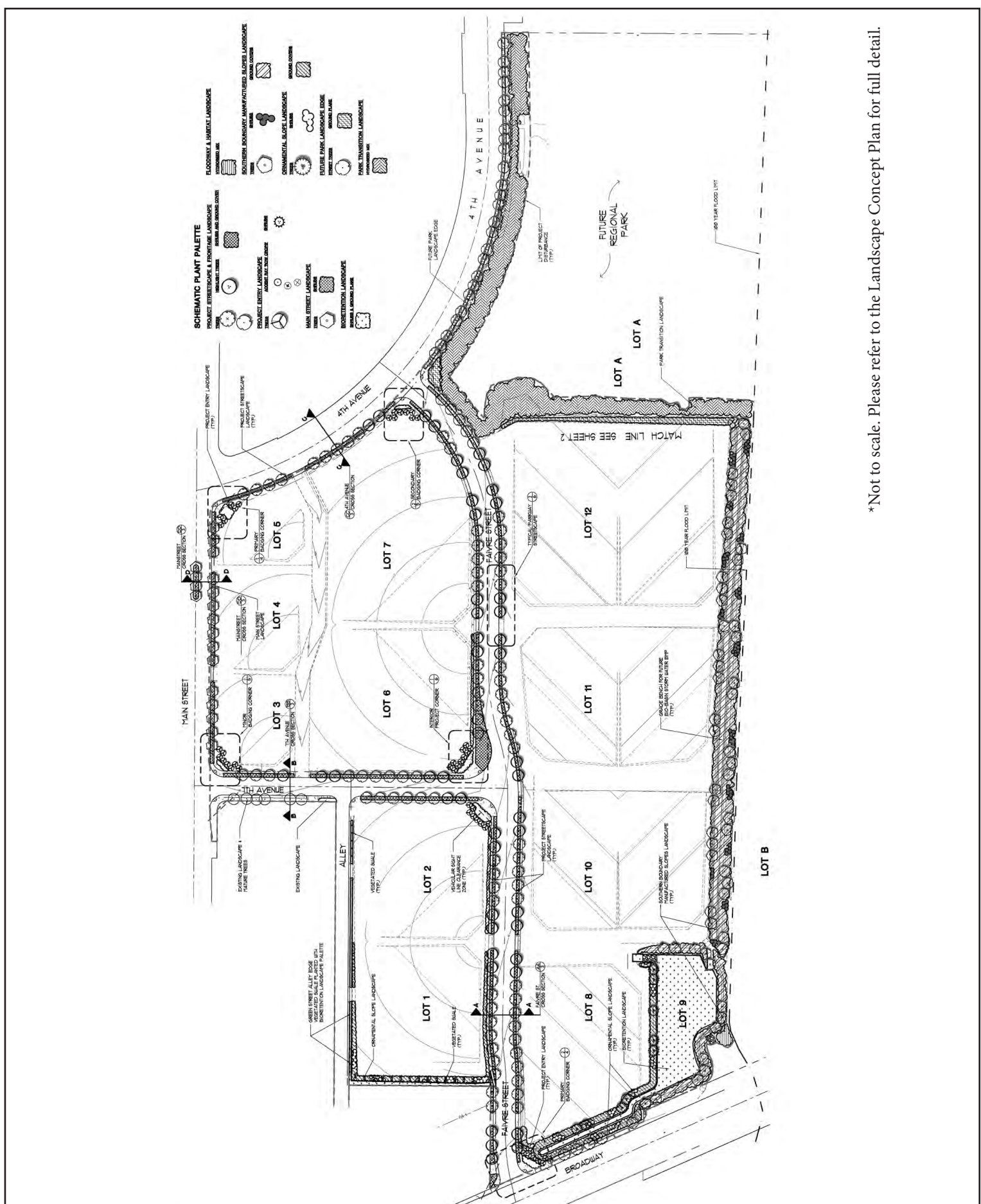
The plant palette has been selected based on drought tolerance and suitability for the local climate, and excludes listed invasive species. Refer to Plant Palette for specific species recommended.

Exhibit 15 Landscape

LANDSCAPE PLANT PALETTE		
Plant Type	Botanical Name	Common Name
Streetscape and Frontage Landscape		
Street Trees	<i>Brachychiton Populneus</i>	Kurrajong
	<i>Lriodendron Tupifera</i>	Tulip Tree
	<i>Lophostemon Confertus</i>	Brisbane Box
	<i>Melaluca Linariifolia</i>	Paperbark
	<i>Pinus Eladrica 'Mondale'</i>	Afghan Pine
	<i>Quercus Virginiana</i>	Southern Live Oak
	<i>Tipuana Tipu</i>	Tipu Tree
Highlight Trees	<i>Jacaranda Mimosifolia</i>	Jacaranda
	<i>Magnolia Grandiflora 'Russett'</i>	Russet Magnolia
	<i>Agonis Flexuosa 'Jervis Bay After Dark'</i>	After Dark Peppermint
	<i>Prunus Cerasifera 'Krauter Vesuvius'</i>	Krauter Purple Leaf Plum
	<i>Eriobotyra Deflexa</i>	Bronze Loquat
Shrubs and Ground Cover	<i>Bougainvillea X 'Monsam'</i>	Golden Jackpot Bougainvillea
	<i>Callistemon X 'Better John'</i>	Better John Dwarf Bottlebrush
	<i>Carissa Macrocarpa 'Tomlinson'</i>	Natal Palm
	<i>Lantana Montevidensis 'Gold Mound'</i>	Gold Mound Lantana
	<i>Nandina Domestica 'Harbor Dwarf'</i>	Dwarf Heavenly Bamboo
	<i>Rhaphiolepis Indica 'Bay Breeze'</i>	Bay Breeze Hawthorn
	<i>Westringia Fruticosa 'Mundi'</i>	Mundi Westringia
	Project Entry Landscape	
Orchard Trees	<i>Arutus Unedo</i>	Strawberry Tree
	<i>Citrus Reticulate 'Clementine'</i>	Clementine Mandarin Orange
	<i>Citrus Limon 'Dwarf Lisbon'</i>	Dwarf Lemon
	<i>Citrus X 'Mineola'</i>	Mineola Tangelo
	<i>Olea Europaea 'Swan Hill'</i>	Fruitless Olive
Accent Ray "Row Crops"	<i>Aloe Striata</i>	
	<i>Bougainvillea X 'Singapore Pink'</i>	Silhouette Bougainvillea
	<i>Helictotrichon Sempervirens</i>	Blue Oat Grass
	<i>Dianella Tasmanica 'Silver Streak'</i>	Silver Flax Lilly
	<i>Dianella Revoluta 'Baby Bliss'</i>	Baby Bliss Flax Lilly
	<i>Croposma X 'Tequila Sunrise'</i>	Tequila Sunrise Mirror Plant
	<i>Echeveria Runyonii</i>	Topsy Turvy Echeveria
	<i>Sedum Spectabile 'Autumn Joy'</i>	Autumn Joy Stonecrop
	<i>Yucca Gloriosa 'Recurvifolia'</i>	Bright Star Yucca
	<i>Aeonium Manriqueorum 'Zwartkop'</i>	Purple Crest Aeonium
<i>Phormium Tenax 'Jack Spratt'</i>	Jack Spratt New Zealand Flax	

Border Hedges	<i>Ligustrum Joponicum 'Texanum'</i>	Texas Privet
	<i>Prunus Laurocerasus 'Otto Luyken'</i>	Otto Luyken English Laurel
	<i>Pittosporum Tobita 'Variegata'</i>	Variegated Mock Orange
	<i>Rhus Ovata</i>	Sugar Bush
	<i>Pittosporum Tennifolium 'Silver Star'</i>	Kohuhu
	<i>Viburnum Borkwoodii</i>	Burkwood Viburnum
Main Street Landscape		
Chula Vista Main Street Specific Trees	<i>Olea Europaea</i>	Olivequer
	<i>Quercus Ilex</i>	Holly Oak
	<i>Pistacia Chinensis</i>	Chinese Pistachio
	<i>Pyrus Calleryana 'Bradford'</i>	Callery Pear
Shrubs	<i>Muhlenbergia Capillaris 'Regal Mist'</i>	Pink Muhly Grass
	<i>Juncus SPP.</i>	Ruhs
	<i>Carex SPP.</i>	Sedge
	<i>Dietes SPP.</i>	African Iris
Bioretention Landscape		
Shrubs and Ground Plane	<i>Dianella Caerulea 'Little Becca'</i>	Little Becca Flax Lilly
	<i>Lomandra Longifolia 'Nyalla'</i>	Nyalla Mat Rush
	<i>Dianella Tasmanica 'Silver Streak'</i>	Silver Flax Lilly
	<i>Seslaria Autumnalis</i>	Autumn Moor Grass
	<i>Seslaria Caerulea</i>	Blue Moor Grass
	<i>Pennisetum 'Eaton Canyon'</i>	Eaton Fountain Grass
Floodway Landscape		
Hydroseed Mix	<i>Encelia Californica</i>	California Encelia
	<i>Eschscholzia Californica</i>	California Poppy
	<i>Lasthenia Californica (Gracilis)</i>	Common Goldfields
	<i>Lupinus Succulentus</i>	Arroyo Lupine
	<i>Mimulus Aurantiacus</i>	Monkeyflower
	<i>Sisyrinchium Bellum</i>	Blue-eyed Grass
	<i>Plantago Insularis</i>	Plantain
	<i>Penstemon Centranthifolius</i>	Scarlet Bugler
Future Park Landscape Edge		
Trees	<i>Quercus Suber</i>	Cork Oak
	<i>Quercus Agrifolia</i>	Live Oak
Ground Plane	<i>Salvia Greggii 'Furman's Red'</i>	Autumn Sage Hybrid
	<i>Salvia Leucantha 'Santa Barbara'</i>	Santa Barbara Sage
	<i>Cistus 'Sunset'</i>	Purple Rockrose
	<i>Mahonia Repens</i>	Oregon Grape
	<i>Arctostaphylos 'Pacific Mist'</i>	Manzanita
	<i>Ribes Viburifolium</i>	Catalina Currant

Southern Boundary Manufactured Slopes		
Trees	<i>Malosma Laurina</i>	Laurel Sumac
	<i>Populus Fremontii</i>	Western Cottonwood
	<i>Platanus Racemosa</i>	Western Sycamore
	<i>Ceris Occidentalis</i>	Western Redbud
	<i>Quercus Agrifolia</i>	California Live Oak
Shrubs	<i>Rhamnus Californica 'Eve Case'</i>	Coffeeberry
	<i>Ribes Viburnifolium</i>	Catalina Currant
	<i>Ceanothus 'Dark Star'</i>	'Dark Star' Lilac
	<i>Salvia Apiana</i>	White Sage
	<i>Rhus Integrifolia</i>	Lemonade Berry
	<i>Epilobium Canum</i>	California Fuchsia
Ground Covers	<i>Encelia Californica</i>	California Encelia
	<i>Eschscholzia Californica</i>	California Poppy
	<i>Lasthenia Californica (Gracilis)</i>	Common Goldfields
	<i>Lupinus Succulentus</i>	Arroyo Lupine
	<i>Mimulus Aurantiacus</i>	Monkeyflower
	<i>Sisyrinchium Bellum</i>	Blue-eyed Grass
	<i>Plantago Insularis</i>	Plantain
Park Transition		
Hydroseed Mix	<i>Eschscholzia Californica</i>	California Poppy
	<i>Acchilea Millefolium</i>	Yarrow
	<i>Gazania Splendens</i>	Gazania
	<i>Fastuca Glauca 'Elijah Blue'</i>	Elijah Blue Fescue
	<i>Trifolium Repens</i>	White Clover
Ornamental Slope Landscape		
Trees	<i>Acacia Stenophylla</i>	Shoestring Acacia
	<i>Rhus Lancea</i>	African Sumac
	<i>Podocarpus Henkelii</i>	Long-leafed Yellowwood
	<i>Eriobotrya Deflexa</i>	Bronze Loquat
	<i>Melaleuca Nesophila</i>	Pink Melaleuca
	<i>Pinus Pinea</i>	Stone Pine
Shrubs	<i>Callistemon Citrinus 'Little John'</i>	Little John Bottlebrush
	<i>Westringia Fruticosa 'Morning Light'</i>	Coast Rosemary
	<i>Rhaphiolepis Umbellata 'Minor'</i>	Dwarf Yeddo Hawthorn
Ground Covers	<i>Juniperus Horizontalis 'Wiltonii'</i>	Blue Carpet Juniper
	<i>Helictotrichon Sempervirens</i>	Blue Oat Grass
	<i>Myoporum Pacificum</i>	Myoporum
	<i>Agapanthus 'Peter Pan'</i>	Dwarf Variegated Agapanthus
	<i>Malephora Lutea</i>	Purple Ice Plant



*Not to scale. Please refer to the Landscape Concept Plan for full detail.



CONCEPTUAL
LANDSCAPE EXHIBIT



OTAY RIVER BUSINESS PARK
SPECIFIC PLAN

Source:
The Lightfoot
Planning Group

H. Maintenance

All required landscape areas shall be permanently maintained by property owners or a Business Owner's Association created for the Specific Plan area. Maintenance includes watering, weeding, pruning, disease and pest control, fertilization, replacement of plant material and irrigation equipment as needed to preserve the health and appearance of site landscaping.

Landscaping shall be maintained so that it does not interfere with vehicular sight distance, conflict with overhead utility lines, overhead lighting or pedestrian lighting and does not block pedestrian or vehicular access.

Irrigation

1. Fully automated irrigation systems will be installed in all areas as required by the provisions of the Landscape Water Conservation Ordinance, Landscape Manual, and The State of California Model Efficiency Ordinance, and shall be consistent with current Stormwater BMP's.
2. Low flow and low precipitation equipment shall supply sufficient water for plant growth with a minimum water loss due to run off and overspray.
3. Irrigation systems shall use high quality, automatic control valves and other necessary equipment for proper coverage.
4. Irrigation controllers shall incorporate 'SMART' technology and the ability to self-adjust based on current environmental factors.

I. Walls and Fences

Parking Lot Screening

Parking areas shall be screened with a landscaped berm, wall, or a solid plant material such as a hedge. If the alternate screening of walls or fencing are used as allowed per Section 4.2 (B), the following criteria shall apply:

1. Height to be at least 3.5 feet but not more than 6 feet. Any walls higher than 3.5 feet must be located beyond the setback area.
2. Solid masonry wall should use landscaping on both sides where feasible to soften the visual appearance and deter graffiti.
3. Open fencing may be allowed if solid planting (hedges) are used in conjunction to provide proper screening at the height specified in 5.2B.
4. Style of fencing should complement building architectural design, and be enhanced with decorative elements such as pilasters.

Between Specific Plan Area and Adjacent Off-Site Commercial/Industrial Properties

Fencing in addition to required parking lot screening is not required, but if included the following criteria shall apply:

1. Walls and fencing with a height up to 9 feet may be allowed, unless in the front setback, subject to approval by the zoning administrator.
2. Either solid or open fencing or walls, or a combination thereof, are permitted, but solid styles should be softened with landscaping where feasible to deter graffiti.
3. Style of fencing should complement building architectural design, and be enhanced with decorative elements such as pilasters. Chain link fencing should be avoided.

Loading Area, Outdoor Storage and Trash Area Screening

1. Walls up to 9 feet, extensions of the building, or wing walls may be used for screening loading areas or trash areas, and should use landscaping where feasible to soften the visual appearance and deter graffiti. Screening visible from the future park area or the Otay River Valley shall use vertical landscape elements to soften the screening walls.
2. Style of walls or fencing should complement building architectural design, and be enhanced with decorative elements such as pilasters. Chain link fencing should be avoided for any area exposed to public view. Chain link fencing with redwood or neutral-colored slating is acceptable for areas not visible to public view.

Between Specific Plan Development and Southern Boundary Slope

Fencing in addition to required parking lot screening is not required, but if included the following criteria shall apply:

1. Walls and fencing with a height up to 6 feet may be allowed.
2. Either solid or open fencing or walls, or a combination thereof, are permitted, but all styles shall be softened with landscaping on both sides to deter graffiti and enhance the natural aesthetics of the nearby Otay River Valley. Native plants are encouraged in these areas.
3. Style of fencing should complement building architectural design, and be enhanced with decorative elements such as pilasters. Chain link fencing should be avoided.

Between Specific Plan Development and Future Park

Fencing in addition to required parking lot screening is not required, but if included the following criteria shall apply:

1. Walls and fencing with a height up to 6 feet may be allowed, subject to approval by the zoning administrator.
2. Either solid or open fencing or walls or a combination thereof, are permitted, and shall be softened with landscaping. Along this boundary, fencing that provides for visibility and a sense of openness is encouraged, such as low fencing or designs with upper areas that are transparent/semi-transparent.
3. Style of fencing should complement building architectural design, and be enhanced with decorative elements such as pilasters. Chain link fencing shall be prohibited. Refer to the OVRP Design Standards and Guidelines, Section 5.3.2, regarding fencing adjacent to the future park area.

J. Signage

Otay River Business Park shall develop a Planned Sign Program with the initial Development Plan review. The Planned Sign Program is to be approved by the Planning Commission of the City of Chula Vista and shall address all project signage, such as but not limited to the following:

1. Entry monument signs
2. Building identification signage
3. Specific business identification signs
4. Directional signage
5. Temporary signage, such as for leasing and sales of businesses

Characteristics to be addressed shall include:

- type (free-standing, wall, etc.)
- size of sign
- overall height
- quantity
- locations to be allowed
- design and material
- illumination

The Planned Sign Program shall adhere to applicable provisions of the CVMC Sign Ordinance: 19.60.005 through 19.60.300, 19.60.600, 19.60.700, 19.60.800, 19.60.810, 19.60.900, and 19.60.930. Additional regulations shall be adhered to as follows: Planning Area 1 shall refer to 19.60.500, and Planning Areas 2 and 3 shall refer to 19.60.580.

K. Trails

Trails in the vicinity of the ORBP include existing and planned trails within the Otay Valley Regional Park. There is an existing OVRP trail segment into the Regional Park within the southwest corner of the Specific Plan at Broadway.

The Business Park design provides transitional vegetation and buffers between the development lots and the Otay River, which are not suitable for additional trail connectivity. The Specific Plan does not provide for any new trails from the Business Park development lots, rather, all trail access will be directed to approved trail locations as encouraged by the OVRP Concept Plan. The Business Park development will provide pedestrian and bicycle circulation along street frontages that will allow for connectivity to this approved trail, providing opportunities for people in the Business Park to readily access the regional park for recreation.

The Future Community Park is anticipated to incorporate trail connections into the OVRP for additional linkages in the area. Specific design and configuration of such trail connections would be addressed as part of the future park planning process, and should follow the OVRP Design Standards and Guidelines for the design of trails in this area.

5.5 Sustainable Design Strategies

- All new structures shall adhere to minimum Tier 1 standards of the CAL Green Code, the State of California Building Code Standards (Title 24).
- Drought-tolerant landscape and low water use irrigation fixtures are required to reduce dependency on potable water.
- Promote water conservation efforts for the Specific Plan area and individual businesses.
- Promote the use of renewable energy by encouraging photovoltaic (solar) panels.
- Encourage all buildings to take advantage of natural ventilation and natural day lighting.
- Encourage building materials to be of recycled content or rapidly renewable, and locally-sourced.
- Promote the use of alternative forms of mobility not dependent on fossil fuels, including the installation of electric vehicle charging stations in parking areas, the provisions of bicycle racks near building entries, changing rooms and showers for bicycle commuters, and the accommodation of scooter and motorcycle parking.

CHAPTER 6 - INFRASTRUCTURE AND PUBLIC FACILITIES

The Chula Vista General Plan's Growth Management Element establishes the policy basis for growth management provisions, and the Public Facilities and Services Element establishes the city's plan to provide and maintain infrastructure and public services for future growth, without diminishing services to existing development. The City of Chula Vista adopted a Growth Management Ordinance in 1991, last updated in 2014, (Chapter 19.09 of the CVMC) to address public services and ensure that development occurs where facilities already exist or that facilities are provided during the development, with standards to ensure that the City can maintain a standard for quality of life issues for residents of Chula Vista. One of the key provisions relative to the western Chula Vista area is to promote revitalization and redevelopment of older portions of the city, and this Otay River Business Park Specific Plan (ORBSP) will implement those goals.

Facility Master Plans / Development Impact Fees (DIF)

The City of Chula Vista utilizes facility master plans to plan for future adequacy of public facilities and services at build-out of the City. They contain assumptions regarding existing and projected land uses and development projections, and identify specific facilities that will serve the build-out of future development, along with phasing and cost estimates. Development Impact Fees and other associated funding programs are established based on the needs identified in the master plans.

To ensure that capital facilities will be constructed for the benefit of new development, Development Impact Fees (DIF) are to be paid for each development project within the city. DIFs are one-time charges that fund capital construction of additional sewer systems, roads, libraries, parks and recreation facilities, etc. made necessary by the presence of new residents and uses. Costs are apportioned to individual units of remaining development by type (residential, commercial, industrial) and are regularly reviewed for potential updates considering changing land uses, costs and/or other circumstances.

Public Facility Financing Plans (PFFPs)

Section 19.09.080 of the CVMC discusses the requirement for the creation of a Public Facilities Financing Plan (PFFP). The ORBSP area is in western Chula Vista, outside of the PC Zone. In this area, the Chula Vista regulatory framework has been established to encourage and manage desirable development and redevelopment, given the unique development challenges faced in this area. Growth management considerations for smaller projects outside of the PC zone are addressed through the CEQA process, which may result in mitigation measures; through conditions of approval, which may require improvements or dedications; and through payment of Development Impact Fees.

This chapter of the Specific Plan is intended to function as an equivalent to a PFFP for the future development in accordance with the ORBP Specific Plan and Tentative Map. Financing mechanisms discussed in Section 6.3 below show that the Otay River Business Park Specific Plan will be responsible for dedications and improvements to facilities that could be affected by the development of a business park on this site. This Specific Plan section identifies the financial responsibility, and financing method for each public facility, and includes a summary of phasing to ensure threshold compliance during project construction.

Development within the ORBP will also pay DIF fees to account for the incremental effect of new development in accordance with the Facility Master Plans.

Fiscal Impact Analysis (FIA)

The industrial and commercial uses within the ORBP will fund and construct for infrastructure needed for those uses and will pay Development Impact Fees to cover incremental costs associated with the new development of this area. As a non-residential project generating jobs, increased property tax and sales tax revenues, the City (through the Development Services Director) has determined that the project will have a positive fiscal impact, and a separate Fiscal Impact Analysis is not required for this Specific Plan.

6.1 Public Facilities and Services

Each of the facilities and improvements necessary for development of the ORBP is discussed in this section. Specific location and design for these improvements are shown on the Tentative Map, to implement these Specific Plan requirements.

A. Circulation and Access

Development of the ORBP will include construction of new streets and improvement of existing frontage streets adjacent to the Specific Plan area, to provide adequate circulation for vehicles, bicycles and pedestrians. A description of these improvements by street is provided below to outline the circulation and access requirements of the Specific Plan. Details of these improvements will be shown on the Tentative Map, and implemented in accordance with the phasing discussed in Section 6.2. All circulation and access improvements shall be constructed as part of the implementation of the Tentative Map when approved with this Specific Plan.

Main Street adjacent to the Site on the northern boundary of the Specific Plan is the primary vehicle access to the area from Interstates 5 and 805. The City of Chula Vista has adopted the Main Street Streetscape Master Plan (MSSMP) for this segment. This is a framework plan for a “Complete Street” - a balanced, connected, safe, and convenient transportation network designed to serve all users regardless of their mode (via walking, biking, and transit, or driving cars or trucks). The plan outlines improved bike and pedestrian connections along Main Street, promotes water conservation by incorporating green street design elements such as curb extensions, medians with drought tolerant vegetation, and shade trees, and establishes a street theme and identity for specific segments of the street. This Specific Plan requires improvements consistent with the Main Street Streetscape Master Plan (<http://www.chulavistaca.gov/departments/development-services/planning>) and improvements to be implemented in conjunction with development of the ORBP are outlined below.

1. An 8.5-foot dedication along the project boundary (south side of Main Street) shall be made to the City of Chula Vista, with a new median and

restriping provided as shown on exhibit 11 to accommodate specified lane widths and medians in the MSSMP.

2. Curb, gutter, and non-contiguous sidewalk, per the MSSMP, meeting ADA standards, shall be provided along the Specific Plan frontage.
3. A 5.75-foot bicycle lane and 2-foot buffer from vehicular travel lanes.
4. Existing electrical boxes at the corner of Main Street and Fourth Avenue shall be relocated in conjunction with site development to an appropriate location on the same corner, in order to accommodate the pedestrian and landscape improvements.
5. Two driveways on Main Street shall be permitted between 7th Avenue and Fourth Avenue, with locations to be approved by the City of Chula Vista Director of Development Services.

Fourth Avenue on the eastern boundary of the Specific Plan area is a secondary access route to the northern portion of the site and will also connect with the extended Faivre Street. Improvements to be implemented by the Specific Plan are outlined below.

1. Curb, gutter, and non-contiguous sidewalk, meeting ADA standards, shall be provided on the west side of Fourth Avenue along the Specific Plan frontage.
2. A 5-foot bicycle lane and 3-foot buffer shall be included on the west side of Fourth Avenue along the Specific Plan boundary.
3. Parking on the west side of Fourth Avenue along the Specific Plan boundary shall be prohibited.
4. One driveway on Fourth Avenue shall be permitted between Main Street and Faivre Street, with location to be approved by the City of Chula Vista Director of Development Services.
5. Improvement of the intersection of Fourth Avenue and Faivre Street shall include a signalized intersection and crosswalks in accordance with the City of Chula Vista standards, to be approved by the City of Chula Vista Traffic Engineering Department.

7th Avenue is currently a public road providing access to adjacent residential and business uses, with limited pavement width and no curbs, gutters or sidewalks. It is to be extended to connect Main Street to the planned Faivre Street with additional improvements as outlined below.

1. Curb, gutter, and non-contiguous sidewalk, meeting ADA standards, shall be provided along the east side of 7th Avenue between Main Street and the proposed Faivre Street extension, and along the west

side of 7th Avenue south of the existing Alley. Along the west side of 7th Avenue between Main Street and the existing Alley, the sidewalk may be contiguous to the curb and gutter due to existing landscape and improvements.

2. Vehicular lanes shall be reconstructed to a minimum of 11-foot wide, with a 6-foot dedicated, striped bicycle lane and 3-foot buffer on each side.
3. Driveway access to Planning Area 1 and Planning Area 2 on the east side of 7th Avenue may be provided as a shared driveway, or as two separate driveways, with final location and spacing to be approved by City of Chula Vista Director of Development Services.
4. Parking along 7th Avenue shall be prohibited.
5. Improvement of the intersection of 7th Avenue and Main Street shall include a signalized intersection and crosswalks in accordance with the Main Street Streetscape Master Plan.

Faivre Street will be a new public street connecting Broadway to Fourth Avenue, creating an east-west access through the Specific Plan site. Improvements to this new extension of Faivre Street to be implemented under the Specific Plan are outlined below.

1. Curb, gutter, and non-contiguous sidewalk, meeting ADA standards, shall be provided along both sides of Faivre Street.
2. Vehicular lanes shall be a minimum of 11-foot wide, with a minimum of 5-foot wide striped bicycle lane on each side.
3. A 12-foot striped two-way left turn lane shall be provided for turning access into properties on both sides of the street to accommodate passenger cars and semi-trucks. Median curbs or other impediments to large trucks shall be prohibited within Faivre Street.
4. The intersection at Faivre Street and 7th Avenue may be an all-way stop.
5. Up to five driveways may be constructed on the south side of Faivre Street, and a total of four on the north side. Final driveway locations are to be approved by City of Chula Vista Director of Development Services.
6. Parking along Faivre Street shall be prohibited.

Broadway on the western boundary of the Specific Plan provides a primary access to the southern portion of the site and to Faivre Street. Improvements to be implemented by the Specific Plan are outlined below.

1. New corner curbs to meet ADA standards and curb, gutter, and contiguous sidewalks shall be provided on the north and south sides of Faivre Street abutting Specific Plan boundary to connect to existing sidewalks.
2. Existing driveways off Broadway into the Specific Plan property shall be closed. A new driveway into the project site south of Faivre Street may be constructed, with final location to be approved by City of Chula Vista Director of Development Services.
3. Improvement of the intersection of Broadway and Faivre Street shall include a signalized intersection and crosswalks in accordance with the City of Chula Vista standards, to be approved by the City of Chula Vista City Engineer.

Alley, existing, is adjacent to part of the northern boundary of the Specific Plan just west of 7th Avenue to Broadway, and will provide secondary access to building pads in the northwest portion of the Specific Plan. Improvements to be implemented by the Specific Plan are outlined below.

1. Reconstruction and repaving of the alley to allow water runoff along the center of the alley shall be provided for the segment adjacent to the Specific Plan boundary and may include the off-site segment west of the Specific Plan boundary (7th Avenue to Broadway).
2. Lots between the Faivre Street extension and the Alley may have access from driveways off the Alley along the Specific Plan boundary.

B. Water Facilities

The project site is within the City of Chula Vista and water service is provided by the Sweetwater Authority, a joint powers public water agency. Existing water lines are located in Main Street, Fourth Avenue, and 7th Avenue to the project site.

Additional water lines shall be provided for water, fire services and irrigation. A water supply, with new and replacement pipes sized for the project buildout estimates, shall be developed along 7th Avenue, the alley, and Faivre Street, connecting to the existing lines on Main Street and Fourth Avenue and creating a looped system. Stubs will be provided for future parcels or buildings. All new water facilities shall be constructed as part of the implementation of the Tentative Map when approved with this Specific Plan.

Exhibit 16 Water Utilities

C. Sewer Facilities

The project site is within the City of Chula Vista Wastewater Faivre Basin 1, and sewer service is provided by the City of Chula Vista. Existing sewer mains are

adjacent to the property on Main Street, and through the Specific Plan property in an existing City of Chula Vista easement. Based on the sewer study provided with the Specific Plan, the existing lines have adequate capacity for the proposed land uses.

New onsite sewer lines shall be provided in 7th Avenue to connect future parcels with these existing facilities. An existing sewer main easement on the site for the City of San Diego shall be maintained, with an addition to the easement per the associated tentative map. All new sewer facilities shall be constructed as part of the implementation of the Tentative Map when approved with this Specific Plan.

Exhibit 17 Sewer Utilities

D. Stormwater and Drainage Facilities

The City of Chula Vista Public Works Department is responsible for ensuring that safe and efficient storm water drainage systems are provided concurrent with development in order to protect the residents and property within the city. City staff reviews individual projects to ensure that improvements are provided which are consistent with the drainage master plan(s) and that the project complies with all City engineering drainage standards. The *City of Chula Vista Subdivision Manual; Engineering Department and Land Development; Section 3, March 2012*, provides design criteria to comply with city design standards.

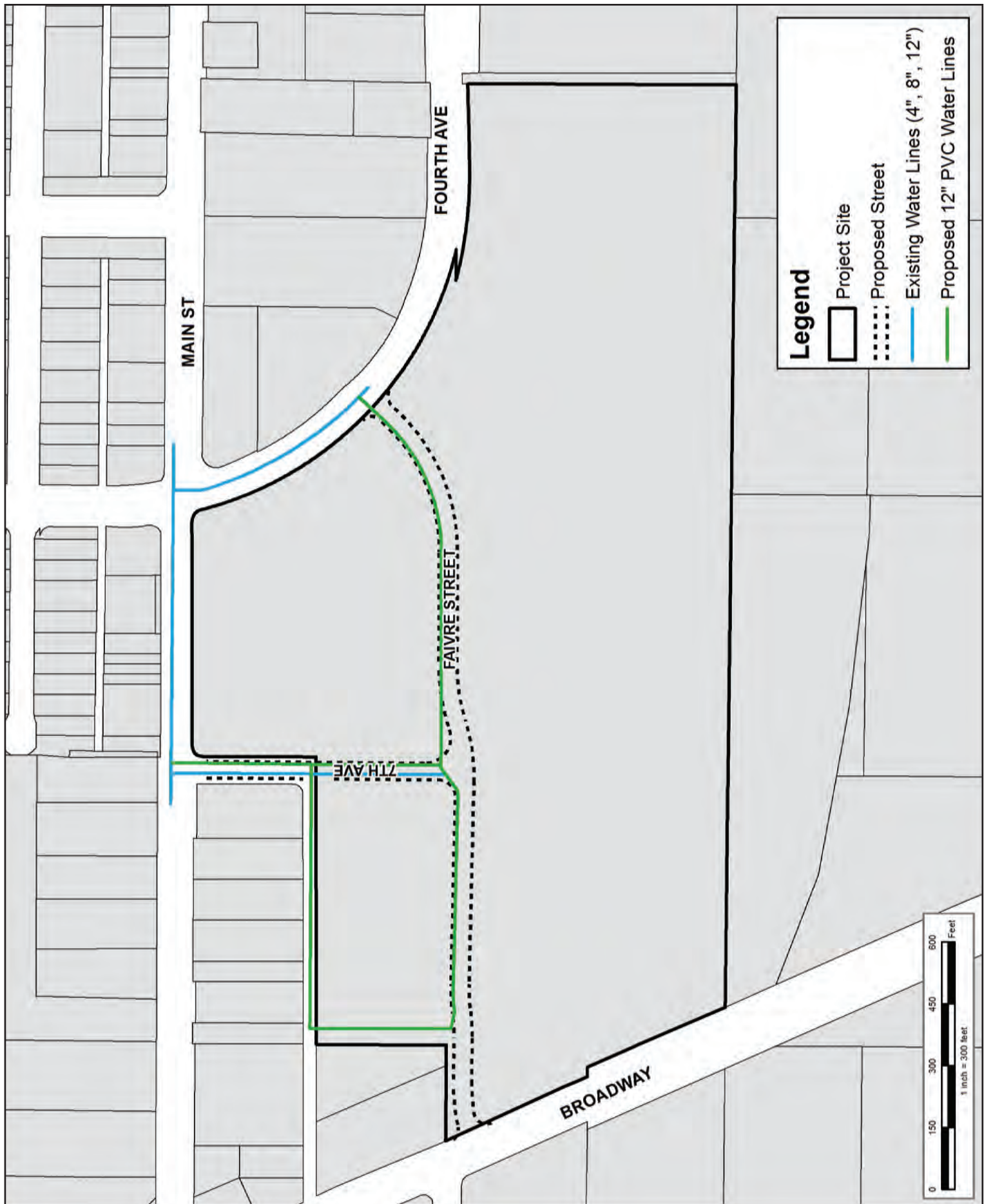
A storm drain system shall be incorporated into the project design that provides treatment for stormwater runoff within the Specific Plan project. This shall include bioretention and desiltation basins as necessary to meet water quality requirements, sized and located according to the proposed mass grading plan and tentative map. These and any future basins, pending approval of a development plan by the City of Chula Vista, shall connect to the existing storm drain outlet at the southwest corner of the property.

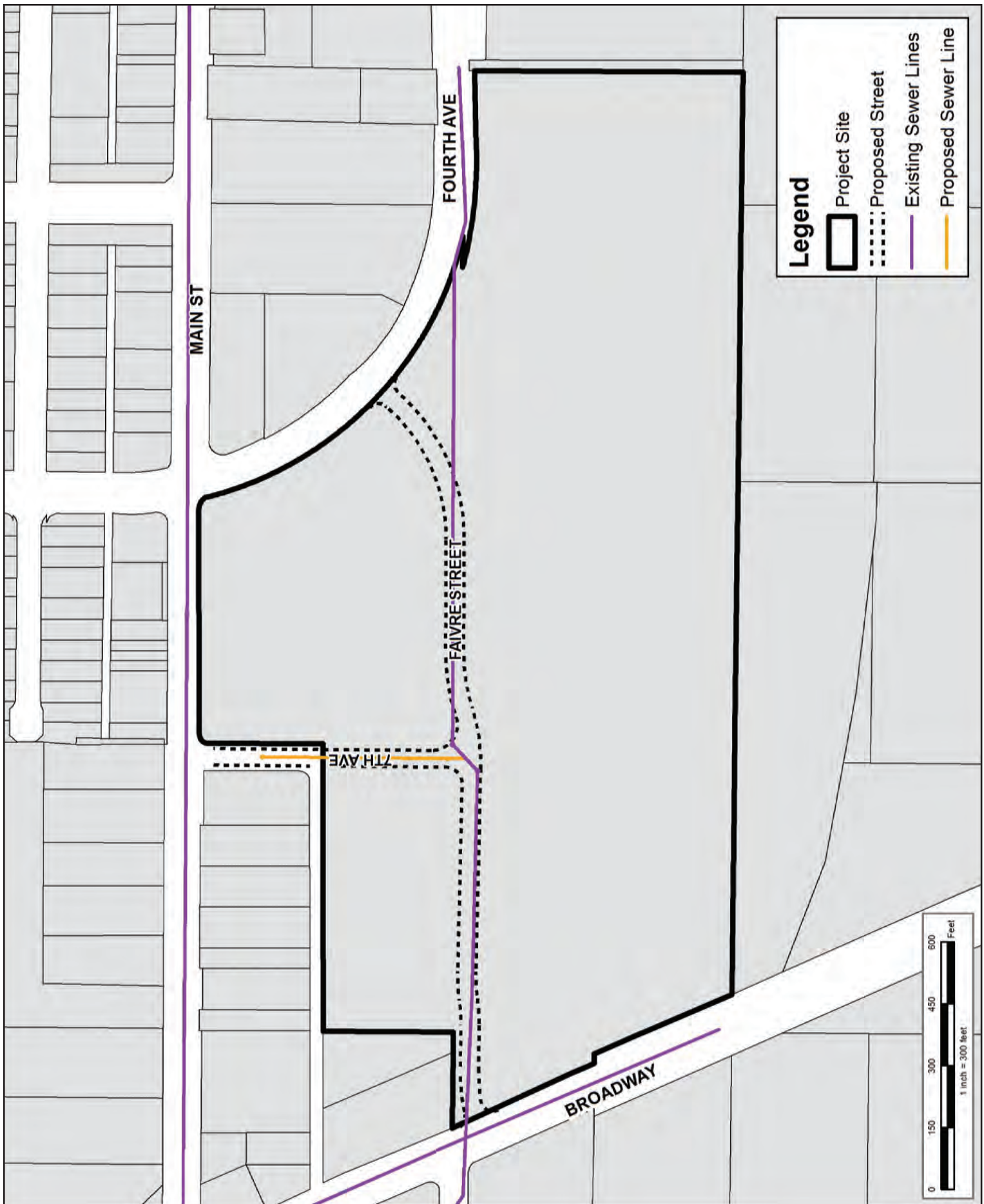
Drainage from the adjacent public streets and the private lots may be combined into common basin(s). Maintenance of all storm water basins within the Specific Plan area, including any future basins located within private lots or within the public right-of-way adjacent to the private property shall be maintained by the owner(s) of properties within the Specific Plan through a Business Owner's Association.

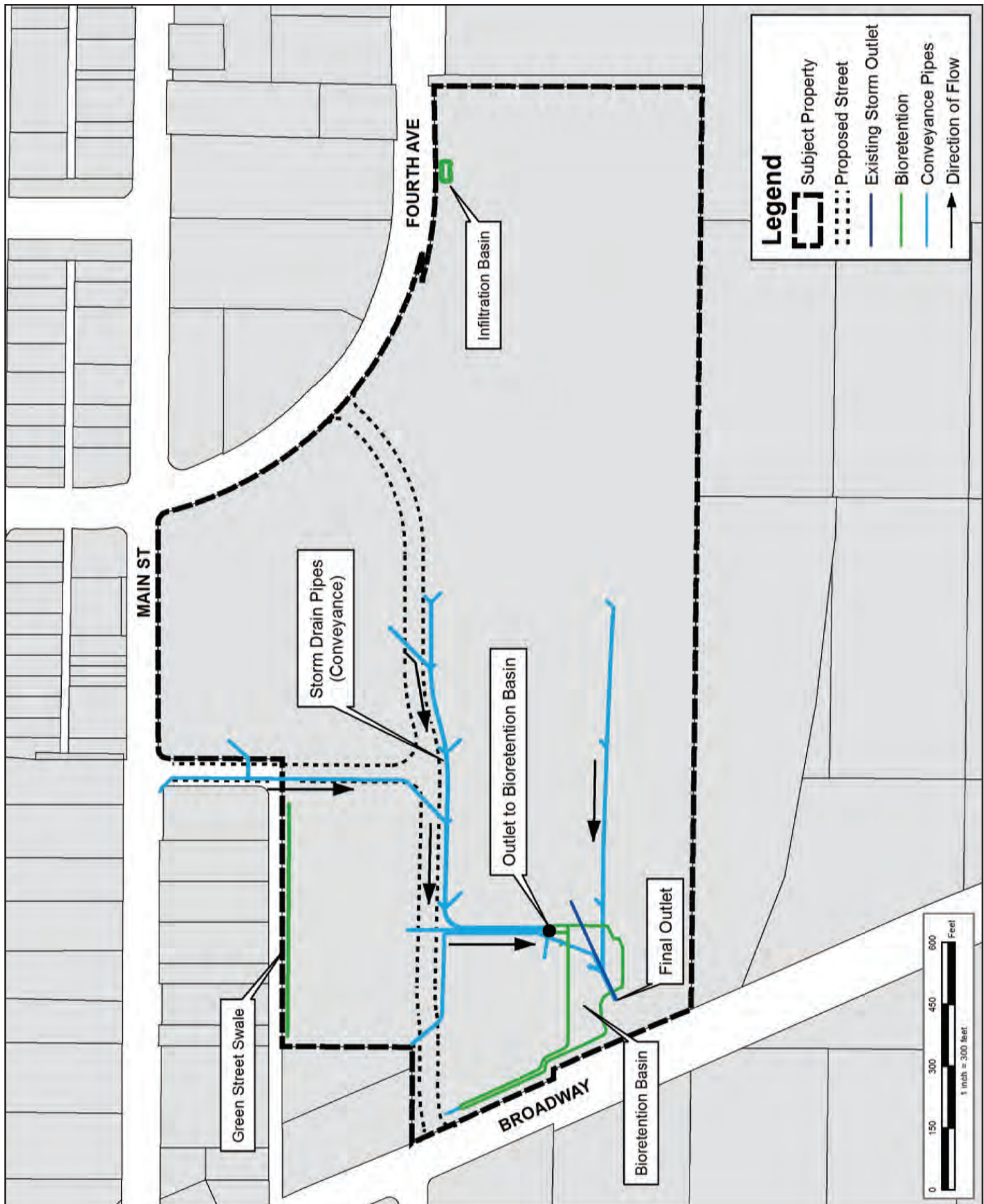
Exhibit 18 Drainage Plan

E. Electricity

Power poles within the Specific Plan boundaries or directly adjacent to the Plan boundaries shall be undergrounded by the Site developer, except where the poles are providing overhead power service to existing uses that need to be maintained.







F. Solid Waste

The City of Chula Vista has franchise agreements for solid waste removal and recycling services. Each building owner or tenant shall be responsible for any fees required for waste and recycling services. Treatment of solid waste shall comply with the City's Recycling and Solid Waste Manual.

G. Public Safety: Police Services, Fire and Emergency Services

Police services will be provided by the City of Chula Vista. The nearest police station to the Specific Plan site is at 315 Fourth Avenue, approximately 3 miles north of the site.

Fire and emergency services will be provided by the City of Chula Vista. The nearest fire department to the Specific Plan site is Fire Department Station No. 5 at 391 Oxford Street less than 2 miles away, with Stations No. 9 (266 E. Oneida Street) and No. 3 (1410 Brandywine Avenue), each located approximately 3 miles away.

Development in accordance with the Specific Plan will provide a new public road (Favre Street extension) parallel to and south of Main Street, between Broadway and Fourth Avenue, along with additional intersection and street frontage improvements that will improve circulation and access for police, fire and emergency response within the development area. As the site develops, the new industrial and commercial uses would add employees, customers and visitors to the site. This could result in an incremental increase in demand for police, fire and emergency services as associated with any new development.

H. Schools

The City of Chula Vista is served by the Chula Vista Elementary School District and the Sweetwater Union High School District. Since no residences are included as part of the Specific Plan, there is no potential for affecting the demand for school facilities within Chula Vista.

I. Libraries

Library services are provided by the City of Chula Vista. No residences are added as part of the Specific Plan that would increase population to affect the public library system of Chula Vista.

J. Parks and Recreation

The project site is not subject to the Growth Management Oversight Commission parks and recreation threshold as it is located on the west side of I-805. However, the General Plan and the OVRP Concept Plan, which the property is within, identify a potential future park site within the vicinity of the subject property. The Specific Plan land uses designate approximately 13.9

acres as undeveloped land available for purchase by the City of Chula Vista for use as a community park.

6.2 Construction Sequence

The initial phase of construction shall be the mass grading and backbone infrastructure for the entire business park. This will include:

- Wet and dry utility installation, including water, sewer storm drain, gas, electric, telephone and cable services
- Street improvements including pavement, curb, gutter, sidewalk and parkway landscaping, with traffic signal installation
- Bioretention basins per the mass grading plan
- Landscaping of perimeter slopes
- Erosion control on the future development lots and future Community Park site

These improvements will allow for flexibility in the sequencing of development on individual parcels. Structure construction will be determined by future proposed development plans and building square footages have not been determined for this Specific Plan.

6.3 Financing Mechanisms

A. Improvement Financing

Several types of financing strategies and tools are available for financing development and necessary infrastructure improvements. Since this Specific Plan area is located at an infill location, no major offsite improvements are necessary at this time, and limited financing mechanisms are needed.

For development within this Specific Plan area, funding for on-site facilities shall be provided directly by the project. This includes improvements within or across adjacent public rights-of way provided concurrent with the project development, such as local street improvements, utility connections from the main trunk lines, and drainage facilities.

The construction of all public street right-of-way (ROW) and utility improvements, private onsite improvements including buildings, parking areas, landscape and hardscape, and amenities serving the project will be the responsibility of the developer/property owner. These improvements will be privately financed.

The Public Facilities Development Impact Fees (PFDIF) dated October 2017 have been used to estimate fees and are subject to change as fees are amended by the City of Chula Vista. Fees based on acreage of proposed land use have been calculated per Specific Plan designations, estimated at 2.9 acres designated commercial (PA-1) and 28.3 acres designated industrial (PA-2 and PA-3). Fees based on building square footages will be calculated when development plans are submitted, as no structures are proposed with this Specific Plan and tentative map. A description of fees for each facility is discussed below, and summarized in Table #1. All fees are subject to change and annual increases and will need verification. Current fees can be determined through the City of Chula Vista (<http://www.chulavistaca.gov/departments/finance/master-fee-schedule>).

Police

The current Police PFDIF are \$8,587/acre for Commercial use and \$1,851/acre for Industrial use, due at the time of issuance of building permits.

Fire and Emergency Services

The current Fire and EMS PFDIF are \$4,014/acre for Commercial use and \$799/acre for Industrial use due upon issuance of building permits.

Schools

California Government Code Section 65995 et. seq. and Education Code Section 17620 et. seq. authorize school districts to impose facility mitigation exactions on new development as a way to address increasing enrollment caused by that development. For non-residential development the statutory fee rates as of September 2017 are \$0.25 in the Chula Vista Elementary School District and \$0.31 in the Sweetwater Union High School District for a combined total of \$0.56 per square foot.

Prior to the issuance of each building permit, the applicant(s) shall pay the School Facility Mitigation Fees in accordance with the fees in effect at the time of building permit issuance, which may be different than those listed above.

Libraries

Financing for the library system is provided by PFDIF fees which are assessed only for residential units. No impact fees are required for commercial or industrial developments, so no funding would be associated with the Specific Plan uses.

Parks and Recreation

Park acquisition and development fees are assessed only for residential units. The Specific Plan includes commercial and industrial uses west of I-805 and is not subject to fees for parks and recreation.

Water

Fees may be collected by the Sweetwater Authority for connection to existing lines, installation of water meters, and installation of fire protection system

connection. Fees will be determined upon approval of development plans for the Specific Plan parcels.

Sewer

Final determination of sewer fees will be calculated at final map, and include but are not limited to the following:

Administrative fees	\$220.00 per Sewer Connection Permit for commercial and industrial
Sewerage Participation Fee	\$3,584 per Equivalent Dwelling Unit
Tap into main, 4" lateral	\$600.00 without City lateral
Tap into main, 6" lateral	\$760.00 without City lateral
Connection Fee	\$16.00 per foot of property frontage

Drainage

All onsite storm water conveyance will be constructed by the developer of the Otay River Business Park. Basins and storm drains to control both the quantity and quality of runoff from the site will be constructed in conjunction with all mass grading activity. Additional requirements for storm water treatment will be designed and built in conjunction with structure design and construction associated with each additional phase. All storm drains within the commercial and industrial areas of the Specific Plan will be maintained by a Business Owner's Association created for and funded by all businesses located within the Specific Plan. Stormwater facilities associated with the future community park would be designed, installed and maintained by the City in conjunction with other park facilities.

Traffic

Traffic Signal fees are collected to fund identified signals city-wide at key intersections, and are based on trip generation at \$38.75 per driveway (total) trips. This fee has been calculated based on Average Daily Trips (ADT) estimated in the Traffic Impact Analysis. Driveway rates (7908 ADT) are based on general land uses of the SP and should be confirmed upon submittal of development plans.

The traffic signal at the intersection of Main Street and 7th Avenue recommended by the Main Street Streetscape Master Plan shall be constructed by the developer, and is eligible for fee credits against the Signal fees. Two traffic signals that are project-specific and necessary for the Specific Plan shall be funded by the developer of the Specific Plan. These are located at the intersections of Broadway and Faivre Street and at Fourth Avenue and Faivre Street.

The ORBPSP site is located west of Interstate 805 in the Western Territory of Chula Vista, and therefore subject to Western Territory Development Impact

Fee of \$68,160/acre for Retail Commercial (specialty retail/strip commercial), and \$38,340/acre for Industrial Park (office/industrial uses clustered into a center, primary is industrial but may include high percentage of other services or retail).

Civic Center

The Civic Center PFDIF current at the time of this Specific Plan are \$9,588/acre for Commercial use and \$3,030/acre for Industrial use.

Corporation Yard

The Corporation Yard PFDIF current at the time of this Specific Plan are \$8,301/acre for Commercial use and \$3,909/acre for Industrial use.

Program Administration

The Program Administration PFDIF current at the time of this Specific Plan are \$2,085/acre for Commercial use and \$659/acre for Industrial use.

Facility	PA-1	PA-2	PA-3	Total
Police	\$25,246	\$19,084	\$33,244	\$77,574
Fire/EMS	\$11,801	\$8,238	\$14,350	\$34,389
Schools	tbd	tbd	tbd	tbd
Libraries	n/a	n/a	n/a	n/a
Parks & Rec	n/a	n/a	n/a	n/a
Water	tbd	tbd	tbd	tbd
Sewer	tbd	tbd	tbd	tbd
Drainage	n/a	n/a	n/a	n/a
WTDIF	\$200,390	\$395,285	\$688,586	\$1,284,261
Traffic Signal	---	---	---	\$306,435
Civic Center	\$28,189	\$31,239	\$54,419	\$113,847
Corp. Yard	\$24,405	\$40,302	\$70,206	\$134,913
Prog. Admin.	\$6,130	\$6,794	\$11,836	\$24,760
Total	\$296,161	\$500,942	\$872,641	\$1,976,179

* The Public Facilities Development Impact Fees (PFDIF) dated October 2017 have been used to calculate fees and are subject to change as fees are amended by the City of Chula Vista and the applicable school districts. Totals include only those fees calculated based on acreage (2.94 acres in PA-1, 10.31 acres in PA-2, and 17.96 acres in PA-3), and are estimates until the submittal of the final map. Fees calculated on building square footages are not included.

B. Maintenance Financing

The recurring maintenance costs for infrastructure and improvements will be provided as follows:

Roadways

Maintenance of vehicular travel ways, curbs, gutters and concrete sidewalks within the public rights-of-way will be provided by the City of Chula Vista and funded by the City's tax revenue. Private driveways within the Specific Plan area and alleys adjacent to the Specific Plan area, which are not maintained by the City of Chula Vista, will be maintained by the property owners or Business Owners' Association (BOA).

Utility Infrastructure

Maintenance of public utilities and facilities except as noted will be the responsibility of the City of Chula Vista (storm drains). Sweetwater Authority maintains water lines. Sewer laterals are maintained privately to the centerline by property owners or a BOA created for the Specific Plan area. Public service utilities contracted to private companies, such as solid waste disposal, will be maintained by the owner of the property. Private utilities, such as electric, gas, telephone, cable television and internet service will be maintained by the respective individual private utility company.

Landscaping

Maintenance of all landscape and hardscape on the project site and in the public rights-of-way, including bioretention basins, Main Street median installed by project, frontage improvements including sidewalks and parkways, walls, and connections to the public sidewalk from the project site, will be the responsibility of the property owners or BOA to be created for the Specific Plan area.

CHAPTER 7 - PLAN IMPLEMENTATION AND ADMINISTRATION

7.1 Introduction

This chapter describes the authority of a Specific Plan, the process that will be used to consider development applications and the administrative procedures required for amendments and/or modifications to the Plan. A Specific Plan is a regulatory tool that local governments use to implement their General Plan and to guide development in a localized area. While the General Plan is the primary guide for growth and development throughout a community, a Specific Plan is able to focus on the unique characteristics of a specialized area by customizing the vision, land uses and development standards for that area. This Specific Plan has been prepared and adopted pursuant to Section 65450 et. seq. of the California Government Code.

7.2 Specific Plan Adoption

The Otay River Business Park Specific Plan shall not be effective unless and until it is adopted by an Ordinance of the City Council. Upon adoption, the Specific Plan implements the adopted General Plan by establishing the land uses, development regulations and design guidelines for the Specific Plan area.

7.3 Specific Plan Administration and Development Review

Development projects within the Specific Plan area shall be subject to a design review process to ensure consistency with the Specific Plan, except as provided below. The Design Review Process is outlined in Chapter 19.14 of the Chula Vista Municipal Code.

All developments within the Specific Plan area require submittal and approval of a Design Review Permit. To be approved, a development project must:

- Comply with the permitted uses and development criteria contained in Chapter 4 - Development Regulations of this Specific Plan, and other applicable regulations contained in the CVMC; and
- Be found to be consistent with the design requirements and recommendations contained in Chapter 5 - Design Guidelines of this Specific Plan and Section 5 of the OVRP Design Guidelines for Private Development.
- Within the Future Park area, development shall comply with the OVRP Design Standards and Guidelines, Section 1-6.

The design review permit will include all conditions of approval ranging from design, environmental mitigation measures, public improvements, and others as may be determined upon review of the specific development project. The design review process will ensure an enhanced level of review for major projects, while minimizing

processing for minor projects, as defined by CVMC Section 19.14.582(g), as may be amended from time to time.

In addition, proposed developments would also be required to adhere to existing CVMC regulations and processes for other discretionary review, such as those for conditional use permits, variances, and subdivisions, as may be applicable. (See 2.55, 19.14, and 19.54, as may be amended from time to time). The provisions of the Zoning Ordinance relative to other discretionary permits or actions (e.g. Tentative Map, Conditional Use Permits) shall be applied as required based on individual development projects.

Permitted land uses within the Specific Plan Area are identified in the Land Use Matrix in Chapter 4. The Development Services Director or his/her designee may determine in writing that a proposed use is similar and compatible to a listed use and may be allowed upon making one or more of the following findings:

- The characteristics of and activities associated with the proposed use is similar to one or more of the allowed uses and will not involve substantially greater intensity than the uses listed;
- The proposed use will be consistent with the purpose and vision of the applicable area;
- The proposed use will be otherwise consistent with the intent of the Specific Plan;
- The proposed use will be compatible with the other uses listed. The Development Services Director or his/her designee may refer the question of whether a proposed use is allowable directly to the Planning Commission for a determination at a public hearing. A determination of the Development Services Director or his/her designee, or Planning Commission may be appealed in compliance with the procedure set forth in the CVMC.

7.4 Existing/Nonconforming Uses

Existing uses that are not listed in the allowable land uses table or determined to be permitted pursuant to the findings and procedure above are, upon adoption of this Otay River Business Park Specific Plan, considered to be nonconforming uses. Such uses shall not be allowed to continue in operation, and shall be abated no later than six months from the date the Specific Plan becomes effective.

7.5 Exemptions

Exemptions to Specific Plan requirements include minor modifications to existing structures such as painting, maintenance or repair, re-roof, modifications that increase the total building area by 200 square feet or less (within a 2-year period) as well as other exceptions and modifications described in CVMC 19.16, as may be amended from time to time.

7.6 Site Specific Variance

Standards contained within the Specific Plan are mandatory requirements that must be satisfied for all new projects and building renovations except where the CVMC Variance regulations (Chapter 19.14.140 - 19.14.270, as may be amended from time to time) provide for a variation from the strict application of the regulations of a particular area.

7.7 Development Exceptions

The land use and development regulations encourage the siting of a variety of land uses in an urban environment that is both pedestrian and environmentally sensitive. To further achieve this goal and promote innovative design, it may be necessary to be flexible in the application of certain development standards. As such, development exceptions may be authorized by the decision making body for the project if all of the following findings are made:

1. The proposed development will not adversely affect the goals and objectives of the Specific Plan and General Plan.
2. The proposed development will comply with all other regulations of the Specific Plan.
3. The exception or exceptions are appropriate for this location and will result in a better design or greater public benefit than could be achieved through strict conformance with the Specific Plan development regulations.

Consideration of a development standard exception shall be concurrent with the review of the Design Review or other permit, as may be required pursuant to Section 7.3 of this Chapter.

7.8 Specific Plan Interpretations

In instances where any section, subsection, sentence, clause, phrase, portion, or word contained within this Specific Plan is undefined, unclear or vague, then the Development Services Director shall make a determination as to its meaning and intent. The Development Services Director or his/her designee may elect to forward interpretations to the Planning Commission for final determination at his/her discretion.

Corrections of typographical or administrative errors found in the Otay River Business Park Specific Plan document shall not require an Amendment to the Specific Plan.

7.9 Specific Plan Amendment

This document attempts to be comprehensive, but it is understood that not all things can be envisioned, and there may be situations where modifications in the Specific Plan may be necessary. The policies presented in the Specific Plan contain some degree of flexibility, but any Specific Plan amendments must be judged by relatively fixed criteria. The California Government Code (§ 65453) states that a Specific Plan “may be amended as often as deemed necessary by the legislative body.” Amendments to this Plan may be initiated by a developer, any individual property owner, or by the City, in accordance with any terms and conditions imposed during the original approval or in accordance with any terms and conditions pertaining to Chula Vista Municipal Code.

The Development Services Director or his/her designee is responsible for making the determination of whether an amendment to the Specific Plan text or maps is needed. The applicant shall be required to submit a completed application with graphics, statements, or other information as may be required to support the proposed amendment. Amendment procedures are described below.

- Proposals to amend the Specific Plan must be accompanied by detailed information to document the change required. This information should include revised Specific Plan text (or excerpt thereof) and revised land use diagram or map amendment, where relevant, depicting the amendment requested.
- Preparation and review of the Specific Plan has been based on comprehensive analysis and best available information at the time of adoption. Any proposals to amend the Specific Plan must therefore document the need for such changes. The City and/or applicant should indicate the economic, social, or technical issues that generate the need to amend the Specific Plan. Costs incurred for the amendments shall be the responsibility of the party requesting the amendment.
- The City and/or applicant must provide an analysis of the amendment’s impacts relative to the adopted environmental review document. Depending on the nature of the amendment, supplemental environmental analysis may be necessary. The need for such additional analysis shall be determined by the City of Chula Vista in accordance with the California Environmental Quality Act (CEQA Guidelines § 15162).

A. Major and Minor Amendments

The Development Services Director or his/her designee shall within 10 days of any submittal of a request to amend this Plan, determine whether the amendment is “minor” (administrative) or “major”. Major amendments (described below) require an advisory recommendation by the Planning Commission and approval by the City Council. If the amendment is determined to be minor, the Development Services Director, or his/her designee, may approve or deny the application. Minor amendments must be determined by

the Development Services Director to be in substantial conformance with the provisions of the Specific Plan and do not include any changes described below for major amendments. Any decision of the Development Services Director, or his/her designee, may be appealed to the City Council, provided said appeal is initiated within 10 working days of receipt by the applicant of written notice of the decision of the Development Services Director, or his/her designee. Examples of "major" amendments include:

1. The introduction of a new land use designation not contemplated in the Specific Plan, as may be amended from time to time.
2. Changes in the designation of land uses affecting two acres or more from that shown in the Specific Plan, as may be amended from time to time.
3. Changes to the circulation system or other community facility which would materially affect a planning concept detailed in the Specific Plan, as may be amended from time to time.
4. Changes or additions to the design guidelines which materially alter the stated intent of the Specific Plan, as may be amended from time to time.
5. Any change which would result in new significant, direct adverse environmental impacts not previously considered in the prior environmental review.

B. Necessary Findings

The Development Services Director, or his/her designee will review the request for Specific Plan Amendment and all submitted supporting material and develop a recommendation on the Specific Plan Amendment for consideration by the Planning Commission and City Council. The Development Services Director or his/her designee may also request further clarification and submittal of additional supporting information, if necessary. The consideration of any proposed amendment to the Specific Plan shall require that the following findings be made:

1. Changes have occurred in the community since the approval of the original Specific Plan which warrants approving the proposed amendment; and
2. The proposed amendment is consistent with the General Plan for the City of Chula Vista; and
3. The proposed amendment will result in a benefit to the area within the Specific Plan; and

4. The proposed amendment will not result in significant unmitigated impacts to adjacent properties; and
5. The proposed amendment will enable the delivery of services and public facilities to the population within the Specific Plan area.